



# *San Marco Avenue* **Design Standards for Entry Corridors**

Approved June 25, 2018



CITY OF  
**ST. AUGUSTINE.**  
EST. 1565







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## Section 1

# PURPOSE & INTENT



Figure 1. Scenes from San Marco Avenue

## SECTION 1. PURPOSE & INTENT

*The purpose of the standards contained in this document is to:*

- a. Promote the **unique characteristics of the San Marco overlay area** that distinguish it from the other built environments of the City of St. Augustine.*
- b. To **facilitate the development of vacant land and the reimagination and repurposing of underutilized properties** in this area into a **high-quality, multi-modal, mixed-use environment**.*
- c. To provide **standards and criteria** by which proposed development in the area will be reviewed.*

### 1.1 Historic Background

The San Marco Avenue Entry Corridor is a unique, neighborhood-oriented shopping, retail, and dining district that extends the pedestrian core from downtown St. Augustine. San Marco Avenue primarily developed between the 1890s and 1930s and stretches from US-1 on the north to West Castillo Drive on the south. The development boom was spurred by the introduction of a street trolley system that ran from Downtown St. Augustine to the Florida School for the Deaf and the Blind, creating a major commercial district just outside of the downtown area.

The mission of the Design Standards is to honor the history of the area and its connections to Downtown St. Augustine as well as to provide and plan for the future evolution of San Marco Avenue.

### 1.2 Neighborhood Context

Although San Marco Avenue is a commercial corridor, the roadway bisects historic neighborhoods, including Nelmar Terrace, Fullerwood, and the Abbott Tract. As the area developed, the residential character once prevalent along San Marco Avenue remains in some areas. Numerous single-family residences dot the Avenue, converted to more contemporary business offerings while maintaining their residential scale and feel.



## 1.3 Two Districts: North and South

**Two major districts** within the corridor have emerged, loosely delineated by the Picolata Road (North/South). **See map in Section 1 for these districts.** Each district contains various defining elements and structures:

### North District (North of Picolata Road)

The north district is a commercial district filled with numerous local eateries and attractions.

*The standards contained herein do not address the public right-of-way, but engagement with the pedestrian sidewalks is a critical element that is addressed.*

### South District (West Castillo to Picolata Road)

The South District is home to a concentrated retail and dining area. This district, the closest to Downtown St. Augustine, exhibits the densest development, with most traditional commercial structures sitting at the sidewalk edge with little to no side separation. The district is also defined by Frame Vernacular residences (many with Victorian ornamentation), some of which are converted to business uses.

## 1.4 Building Design

Traditional commercial and residential typologies define the corridor. The standards that follow herein allow for infill of these two architectural typologies, which will help bring more attention to the iconic pieces of architecture defining the main Institutional users: the Florida School for the Deaf and Blind, the Mission Nombre de Dios Shrine, the City of St. Augustine Waterworks building, and the Mark Lance Armory (Florida National Guard).



Figure 2. Scenes from San Marco Avenue

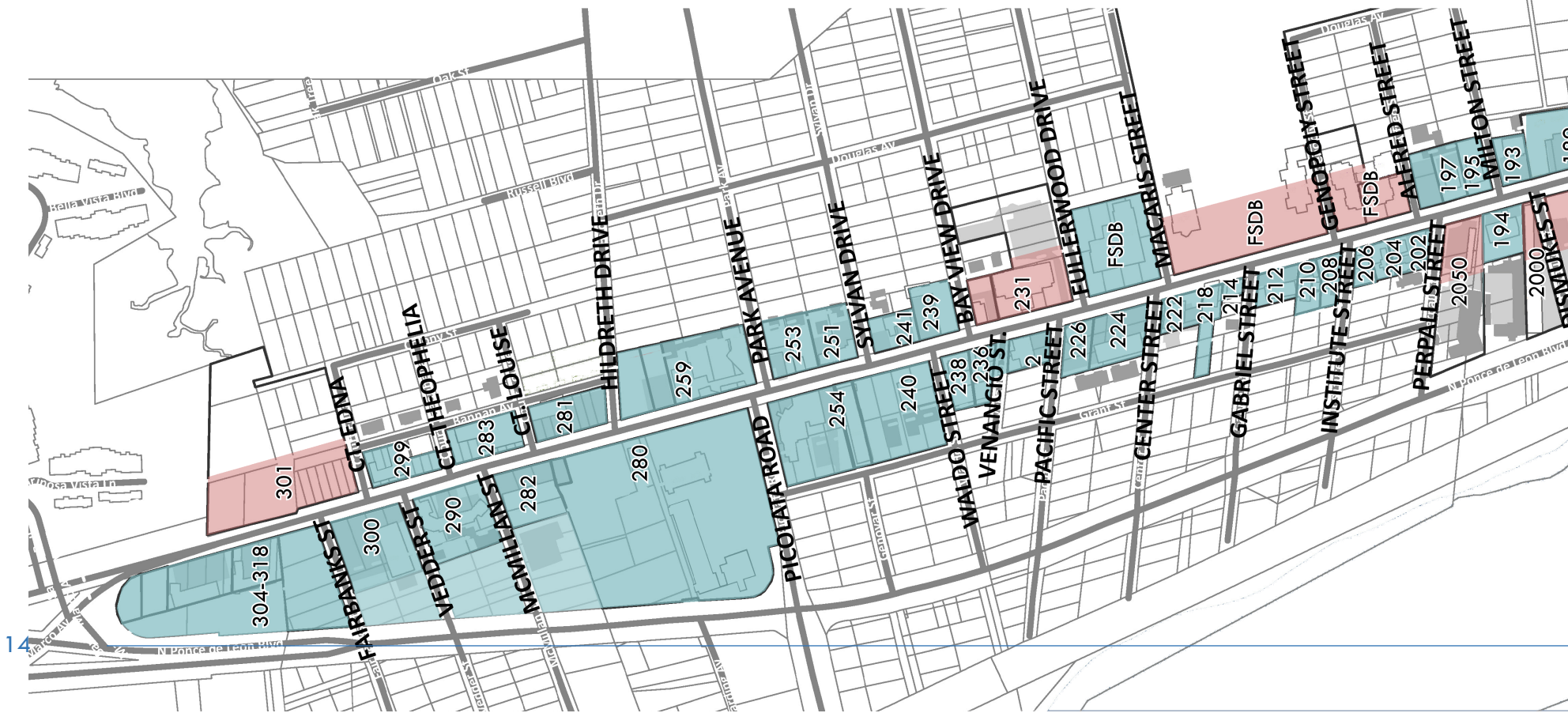


## 1.5 EXTENTS OF SAN MARCO ENTRY CORRIDOR SITES

### Criteria:

- 1.) Sites with frontage along San Marco Avenue and/or US 1 (east) north of Picolata Road.
- 2.) 150' from the San Marco Avenue right-of-way shall be applicable.
- 3.) If 150' is over 70% of entire parcel, all of parcel is applicable.
- 4.) Accessory uses and structures, included but not limited to parking on adjacent or nearby sites, shall be subject to the San Marco Avenue Design Standards.

The map of sites ( indicates in blue if entire parcel falls under the Design Standards, or red if 150' impact zone is applicable.



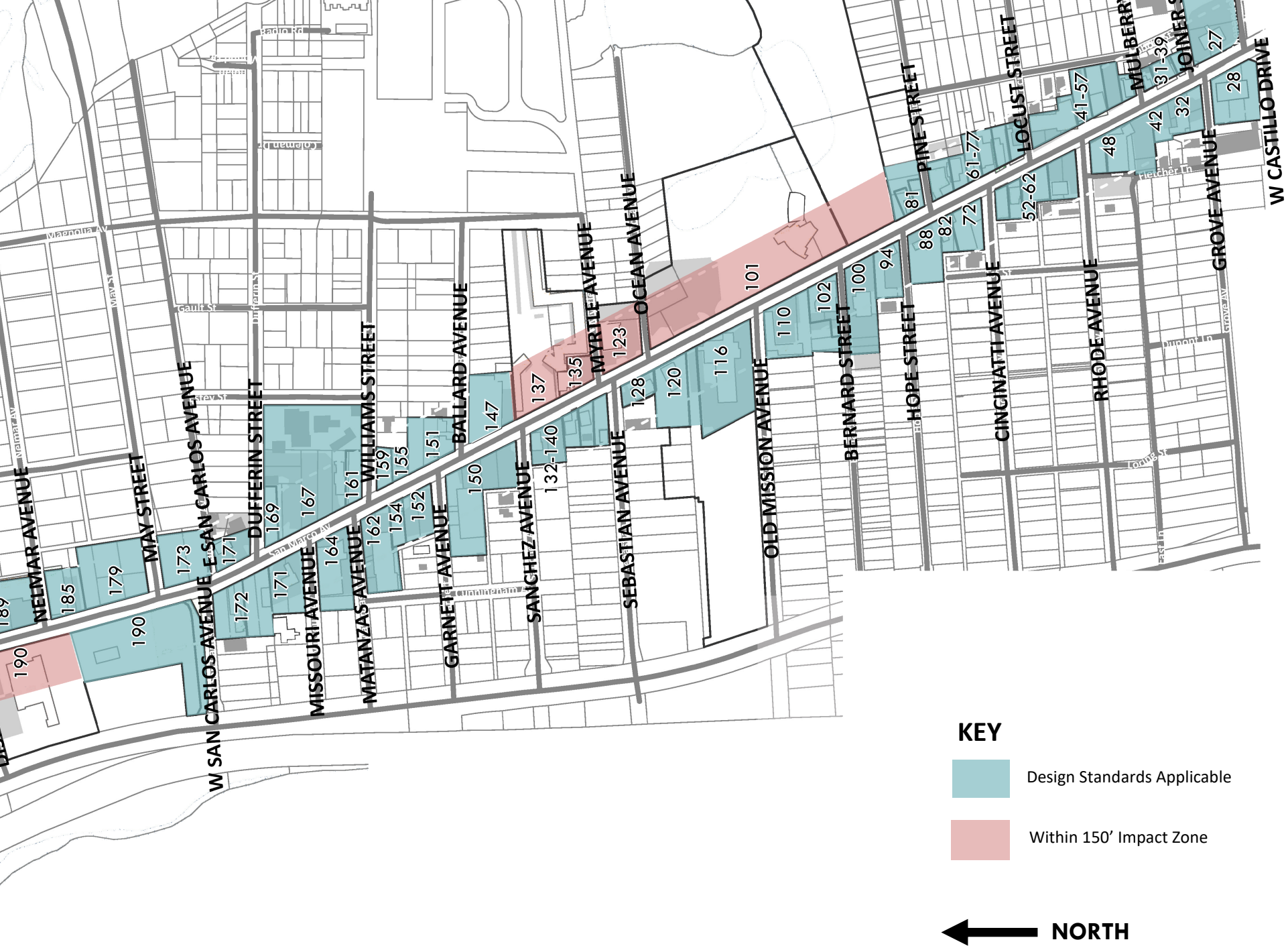


Figure 3. Map of San Marco Entry Corridor



"FAIRLAWN" GUEST HOME  
U. S. ROUTE 1 — 94 SAN MARCO AVENUE  
ST. AUGUSTINE, FLORIDA







## Section 2

### WHEN THE STANDARDS APPLY



## SECTION 2. WHEN THE SAN MARCO AVENUE ENTRY CORRIDOR DESIGN STANDARDS APPLY

The San Marco Avenue Design Standards **shall apply to new development and redevelopment** on sites abutting San Marco Avenue between West Castillo Drive and the Highway US 1 Interchange and the northern portion of US-1 (see *Section 1.5*). The following subsections specify the level of compliance required based on the development activity proposed.

The standards are in addition to the requirements of the City's comprehensive plan, zoning, and land development regulations found in the City's code of ordinances. When there is a conflict the Standards will apply when legally applicable. **These standards apply to parcels fronting the San Marco Avenue corridor.** Some parcels are quite deep and/or extend beyond adjacent sites; when noted on Section 1.5, only the first 150' measured from the front property line is regulated by these standards. **The public right of way (ROW) is not included in these standards.**

The standards herein are based on the *existing conditions* of the public right-of-way on the date of adoption. In the event that the public right-of-way changes, standards shall still be applicable unless technically infeasible, as presented by the Applicant and determined by the CRC. **While specific building massing and position on the site may shift due to these standards, the general building mass allowed is per the City's comprehensive plan, land development code, and ordinances.** The intention of this document and all regulations set forth herein is for **no net loss of buildable square footage** on any site along the San Marco Avenue Entry Corridor.

More specifically, these standards apply in the following conditions:

1. **New Construction.** All new development along the San Marco Avenue corridor, including new buildings or structures on vacant or already developed sites, shall comply with all the regulations contained in this document.
2. **Substantial Expansion or Redevelopment of Existing Buildings.**
  - a. **Full compliance.** An entire development shall be brought into *full* compliance with this section if the redevelopment constitutes a substantial improvement as required by city code.
  - b. **Exceptions.** The following shall be the only exceptions to full code compliance for substantial expansion or redevelopment:
    - i. **Building setback.** Existing buildings will not be required to be moved or expanded to meet the setback requirements. However, building additions shall meet the required setback. All *new* buildings within the redevelopment site shall be required to meet the building setback provisions where legally applicable. *Note:* For Legal Non-conforming Uses: A building which has been damaged may be repaired or reconstructed and used as before the time of damage, provided that such repair or reconstruction is substantially completed within twelve (12) months of the date of such damage (see Sec. 28-118).



- ii. **Building frontage.** Existing buildings shall not be required to meet the minimum building frontage requirement. However, new buildings and additions shall be required to comply with the frontage requirements **as per Section 4.**
  - iii. **Driveways and Parking.** See *Section 4.5.4* for allowed exceptions to existing driveways and parking facilities.
  - c. **Other LDC Provisions.** Compliance with parking, landscaping, screening and all other regulations stated in the city's land development regulations and not addressed in this document shall be addressed as required by the City's land development regulations.
- 3. **Non-Substantial Additions or Redevelopment of Existing Buildings:** For proposed building additions or redevelopment not meeting the criteria of Subsection 2.A, above, only the new additions or exterior building modifications shall comply with the regulations contained in this document. The remainder of the building and the site shall not be required to meet the standards.
  - 4. **Cumulative Improvements:** Site and building improvements not subject to the building code (design/aesthetic improvements) that exceed 50% of the building value over a five year period will be deemed a substantial improvement for the purposes of the Design Standards and subject to review for compliance.
  - 5. **Change in Use.** Changes in use (to any other permitted use in the zoning district) without any physical improvements to the site are not subject to the San Marco Avenue Design Standards.
  - 6. **Non-Conforming Uses:** Any changes to non-conforming uses, including signage, shall be conducted per the requirements of the City's land development regulations (Chapter 28, Division 4, Nonconforming Lots, Uses and Structures).
  - 7. **Ancillary / Accessory Uses and Structures:** Accessory uses and structures, included but not limited to parking on adjacent or nearby sites, shall be subject to the San Marco Avenue Design Standards.
  - 8. **Interior Changes:** Interior changes shall not be subject to the requirements of this document.
  - 9. **Corporate Architecture:** All national and international corporate commercial buildings shall abide by the San Marco Avenue Design Standards, as applicable by law. See *Section 3.7.2*.
  - 10. If a provision of these Standards is or becomes illegal, invalid or unenforceable, that shall not affect the validity or enforceability of any other provision of these Standards.

Note that Planned Unit Development zoning on parcels within the corridor may require adherence, variation from, or full exclusion from these standards. Refer to individual PUD documents for details.







## Section 3

# APPLICATION REVIEW PROCESS



## SECTION 3. SAN MARCO AVENUE DESIGN STANDARD APPLICATION REVIEW PROCESS

The zoning and building codes regulate land uses, construction and signs. Developers must obtain a permit from the planning and building department prior to commencing work or installing signs. Projects involving land disturbance may require investigation by the city archaeologist prior to work.

### 3.1 Application Process

Application forms and instructions may be obtained on the City's website. Applications must be signed by the property owner OR design professional. An application fee is required. Supporting documentation, such as plans, elevations, sketches, photographs, samples of materials and specifications shall be submitted with the application.

### 3.2 Plan Review

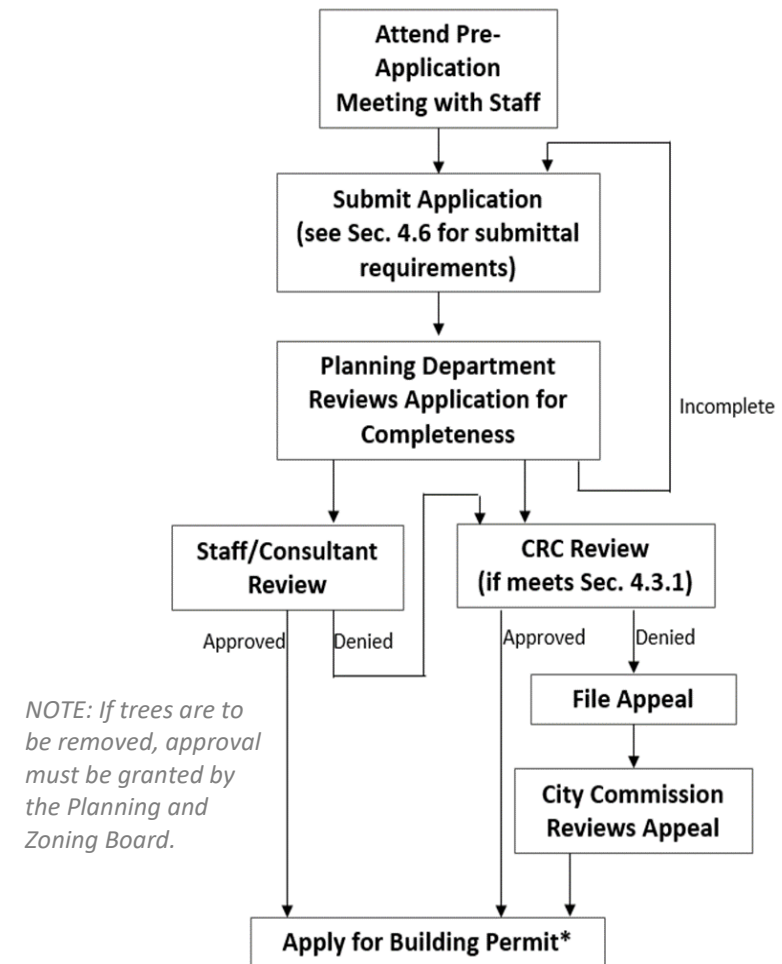
The Planning and Building Director or designee shall be responsible for reviewing development plans to determine compliance with the Anastasia Boulevard Design Standards.

### 3.3 Corridor Review Committee

#### 3.3.1 Activities Requiring Review.

*Any of the following items are a triggering event and requires further review by the CRC.*

- Modification of standards between 10 and 30% of a dimensional requirement of this document and non-dimensional modifications (color, landscape, etc.). Changes over 30% cannot be approved by the CRC, but may be processed through the variance process (Sec. 28-29).
- Limits of Construction that exceed one third (1/3) acre in size;



\* Not including other reviews such as zoning exception or variance.

Figure 4. Review Flow Chart





- c. Limits of Construction that exceed 2,500 square feet in gross floor area;
- d. New construction that includes structured parking;
- e. Use of neon or color lighting (non-white lighting, outside of 2700K-3000K range, requires review);
- f. Colors not pre-approved;
- g. Appeals of Planning and Building Department of administrative official.

### 3.3.2 Review Criteria.

The CRC shall use the following criteria (in addition to the standards contained herein) when reviewing an application:

- a. The proposed project meets the intent of the San Marco Avenue Design Standards as defined in Section 1;
- b. Approval of the proposal will not set an unintended precedent; Note: Unique qualities of the application will be identified to substantiate the approval and avoid a perceived assumption of precedence.
- c. Approval of the request will not be detrimental to the physical characteristics of the neighboring sites or the overlay district as a whole, with respect to the physical characteristics prescribed within the authority of this document.

## 3.4 Effect of Approval.

If the application is approved by the planning and building department staff (or designated entity), the applicant may proceed to apply for a building permit. If the proposed use requires a zoning exception or variance, the applicant shall submit to the Planning and Zoning Board. One courtesy concept review may be submitted for review prior to application submittal but final approval is only granted with approval of an application and issuance of a building permit.

## 3.5 Appeals

Staff determinations may be appealed to the CRC. CRC determinations may be appealed to the City Commission.

Applicant appeals must be filed by an aggrieved person within 30 days of the action being appealed whether it is an administrative decisions or CRC decision. Appeal should state item number, address, action and justification for reversal or modification. Note: the appellant must have legal standing.



## 3.6 Submittal Requirements.

See Application Checklist in Appendix.

For signage and painting, submittals shall be provided by a professional and/or the property or business owner.

For building modifications requiring building permits, all plans shall be prepared by appropriate registered professionals including but not limited to; architects, landscape architects and engineers.

Plans submitted must be neat, legible, drawn to scale and dimensioned. Failure to submit complete plans will result in the plans being returned as incomplete. An electronic submission is required. The plans shall contain documentation for the scope of work and may contain the following:

All Submittals shall include:

- A list of any requested modifications of standards.
- A clear and detailed description of all proposed construction.
- Photographs of the existing structure (pre-construction) and all adjacent structures.

Basic Submittals (submit items from “All Submittals” and applicable items from below):

*A Basic Submittal is required for landscape, signage, and exterior modifications not deemed Substantial Improvements (see definition of Substantial Improvements below).*

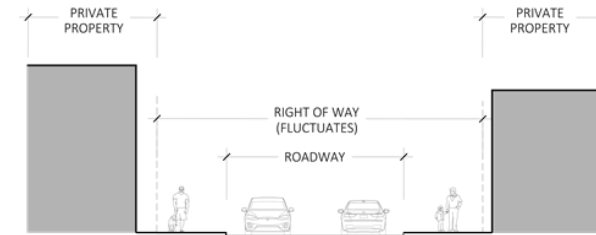
- Paint colors and locations (including paint chips or samples).
- Light fixtures and photometric plan for site lighting over 8’ in mounting height
- Fences and Street walls (height, materials and colors)
- *Separate application form for a signage— only or landscape-only submittal may be found on the City’s website.*

Advanced Submittals (submit items from “All Submittals”, applicable items from “Basic Submittals,” and applicable items from below:

An Advanced Submittal is required for *Substantial Improvements* meaning site and building improvements not subject to the building code (design/aesthetic improvements) that exceed 50% of the building value over a five year period OR for *New Construction*.



- A complete site plan of the property to be developed, showing the location of all structures and buildings, required yards, required parking, surface drive areas, loading spaces, stacking spaces, landscaping areas (both perimeter and interior), the location of all trees three (3) inches or larger dbh, by species and dbh (both to be removed and to be retained), dumpsters, exterior mechanical equipment, storm drainage retention areas, etc. (scale preferred: 1 inch = 20 feet).
- Landscape planting plan at a minimum scale of 1 inch=20 feet including hardscape, plant materials, sizes and location.
- Elevations of all proposed construction (scale preferred: 1/8 inches = 1 foot).
- Wall sections (scale preferred: 3/4 inches = 1 foot).
- Northerly and easterly sections through the site focusing on the proposed building(s), existing building(s), and sidewalks. All abutting roadways should be shown. Preferred scale: 1/8 inches = 1 foot.
- Images presenting the structure's mass and form in relation to its surrounding structures must be included for all new construction. A minimum of two drawings, must include parcel surroundings plus cross street adjacencies. (Tools such as SketchUp, which is free and readily available, is recommended). Any documents needing to be *signed & sealed* shall be produced by a licensed professional.
- Details indicating the following (scale preferred: 1-1/2 inches = 1 foot) windows, shutters and shutter hardware
- Exterior surfaces, materials and textures
- Chimneys
- Exterior porches, landings, stairs, ramps, railings and banisters
- Walks and drive surface materials
- Patios and decks
- Other miscellaneous ornamentation (for Corridor Representative Example)
- Renderings, optional (mandatory for new construction)
- Advanced submittals shall be submitted electronically on 24"x36" (or 22"x34") sheets



*Example of section through the site*



## 3.7 Modification of Standards

Due to the individual unique characteristics or circumstances of any given development along San Marco Avenue, flexibility in the application of design standards may be warranted in certain situations. Modifications from the standards may be requested by an applicant as part of the development review process. If an applicant requests multiple modifications, each modification shall be evaluated independently.

### 3.7.1 Application.

All requests for modifications shall be noted on the site plan. The request shall be approved or denied during development plan review and, if granted, shall be noted on the final development plan.

### 3.7.2 Administrative Modifications.

The Planning and Building Department Director or designee shall have the authority to grant limited modifications, as set forth below, where it is determined that the proposed development meets the intent of the zoning district, the requested modification is the minimum necessary to allow reasonable development of the site and the requested modification is not injurious to the public health, safety and welfare. Administrative modifications are limited to ten (10) percent of the dimensional requirements established in this document. An applicant that meets the statutory criteria of Section 553.79(20)(a), Florida Statutes, must clearly identify in the application materials the specific elements of the proposed project that apply to the statute. Once identified, these statutory design elements will be approved administratively.

### 3.7.3 CRC Modifications.

The CRC shall review the following modification of standards:

- a. Modifications of more than ten (10) percent but no more than thirty (30) percent of a dimensional requirement,
- b. Modifications of ten (10) percent or less previously denied or referred to the Corridor Review Committee by staff, and
- c. Modifications to the non-dimensional requirements contained in this document.





### 3.7.4 Prohibited Modifications.

No modification shall be granted under this section for the following:

- a. Use of property as defined in Land Development Regulations.
- b. Maximum levels of density or intensity allowed in comprehensive plan.
- c. Encroachments into the public right-of-way.
- d. A reduction of a setback to less than 5 feet adjacent to a single family residential district.

### 3.7.5 Review Criteria.

A modification of standards may be granted by the approving authority if it finds that strict application of the standards is not warranted and that granting a modification will fulfill the intent of the standards. The approving authority shall apply all the following criteria, when applicable, to determine if the applicant has justified a request for a modification:

- a. The request is within the parameters listed above.
- b. The request is consistent with the Comprehensive Plan and generally consistent with the purpose of the city's Land Development Regulations and this document.
- c. The proposed modification will not have a material negative impact on adjacent uses, or the applicant proposes to mitigate the negative impact to be created by the modification.
- d. Compliance with the requirement is technically infeasible based on site conditions.

### 3.7.6 Additional Requirements.

- a. The burden of presenting evidence sufficient to satisfy the applicable criteria remains with the applicant seeking the modification.
- b. The applicant may propose conditions to ensure that the use of the property to which the modification applies will be reasonably compatible with the surrounding properties, including visual screening.
- c. A granted modification and any conditions attached to it shall be entered on the face of or attached to the development order or permit and development plan.
- d. A proposed change to an approved modification shall be added to the approved modification and considered in the aggregate. The total modification (approved modification plus proposed change) shall determine the approving authority per *subsections 3.7.2 and 3.7.3*, above.



12 HOTEL SAN MARCO, ST. AUGUSTINE, FLA.



## Section 4

# SITE & BUILDING DESIGN



## SECTION 4. SITE & BUILDING DESIGN

The rhythm and pattern of buildings, combined with their relationship to the street expressed through setbacks and architecture, define the character of the public space. The location of a building on a site is critical to creating a vital and coherent public realm that is both welcoming and functional. This public realm shall also be determined by the location of the property along the corridor (and its surroundings) and meeting the following site and building design standards.

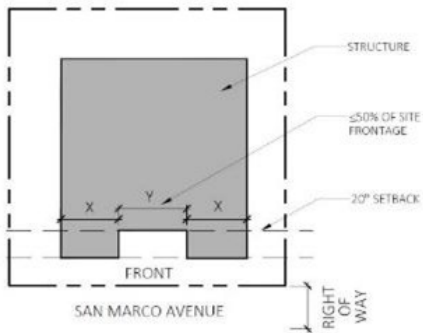


Figure 5. Expanded Front Yard Setback for Courtyard

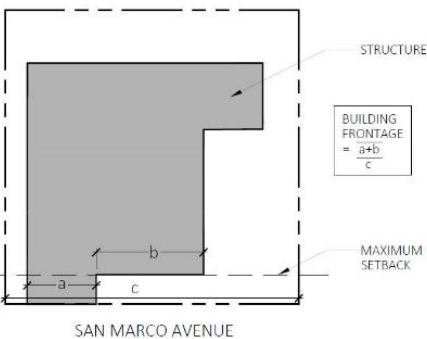


Figure 6. Measuring Building Frontage

Only the "a" and "b" facades are required to meet the maximum building setback, assuming that  $a + b$  meet the frontage requirement of Section 6.1 (Building Frontage).

### 4.1 Building Placement

The setback standards prescribed in this section are intended to create a pedestrian-friendly public realm with buildings located close to the street (at varying degrees). The public realm typically includes all the elements within the right-of-way (sidewalks, travel lanes, on-street parking, landscape strips), but it also includes activities that occur within the front yard setbacks, which in a commercial area are considered to be the transition area between the public and the private spaces and needs to be inviting for the commercial corridor to be successful. That transitional zone in the public realm may also include expanded sidewalks, outdoor cafés, bicycle parking, art/sculptures, and outdoor retail spaces, among others. To encourage the creation of a successful public realm, the following building placement standards shall be met. All *new* buildings within the redevelopment site shall be required to meet the building setback provisions where legally applicable.

Table 1. Building Setbacks (NOTE: Setbacks are measured to the building overhangs/eaves)

#### City of St. Augustine Entry Corridor Setbacks

	Front Yard	Upper Floor Setback	Side Yard	Street Side Yard	Interior Side Yard	Rear Yard Setback	Setback adjacent to Single Family Zoning
San Marco <b>Residential</b>	15' min, 30' max			5' min	5' min	5' min	15' min
San Marco <b>Commercial</b>	0' min, 10' max			0' min, 10' max	0' min	5' min	15' min
San Marco <b>Institutional</b>	10' min, NO max			10' min	10' min	10' min	15' min





1. For those corner lots abutting a R.O.W. intersection, the visibility triangle must be kept free of visual obstacles. Visibility triangle regulations shall always prevail. See *Section 5.10*.
2. New Commercial Type developments adjacent to a development already using a 0-foot side yard setback shall provide a side yard setback of 0 feet.
3. There shall be no parking or drop off areas in the front yard of any of the commercial or residential typologies allowed unless approved by the CRC. Institutional buildings are exempt from this regulation.
4. The maximum front setback for the Commercial Typology may be expanded to 20 feet for a maximum of 50% of the building frontage (as defined in 4.2a) for use as a public courtyard, landscape, or other non-parking purposes. (see *Figure 5*).
5. The maximum front and street side building setbacks (if side street setbacks exist) shall only apply to the portion of the building meeting the building frontage requirement (see *Figure 6*). This provision will allow for L-shape buildings or step back setbacks, where a portion of the building is recessed more than what the setback allows.
6. The front and side setbacks may be used to expand the public sidewalk, for outdoor dining, or for landscaping (see *Section 5*).
7. Balconies, bay windows, awnings, canopies, eaves and roof overhangs used in conjunction with a commercial structure may encroach into the setback zones, but not into the public right-of-way.
8. Porches in the Residential Typology can encroach into the minimum front yard setback up to 8 feet.
9. New developments or additions abutting or from a single-family residential zoning district shall provide a fifteen (15) foot setback along the portion of the site abutting or from the single-family zoned property. Porches, however, can encroach upon this setback up to eight (8) feet. Landscaped buffers shall be provided as required in *Section 5* of this document.
10. Primary building facades facing a right-of-way shall be built parallel to the property line along that street (see *Figure 7*).
11. Existing buildings on a site subject to redevelopment will not be required to meet the setback standards listed above. However, if technically feasible and not detrimental to the architecture of the existing building, any additions to the building shall serve to bring the building closer into compliance (see *Figure 6*).

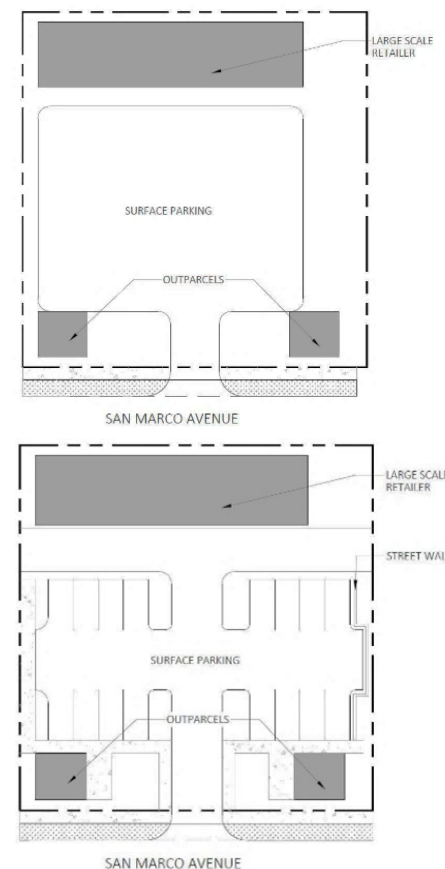


Figure 8. Multiple Buildings on a Site

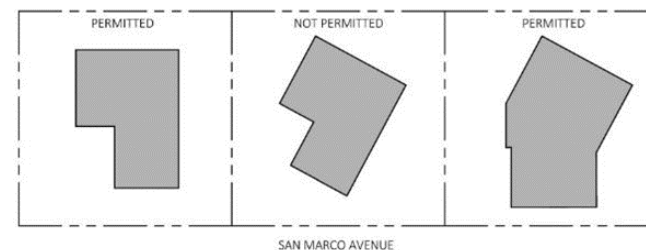
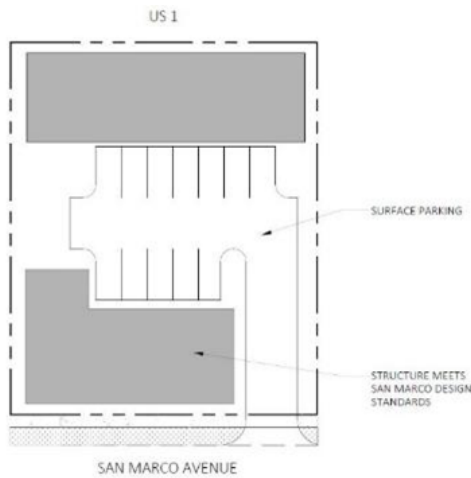


Figure 7: Building and Street Alignment



## City of St. Augustine San Marco Avenue Design Standards



**Figure 9. Multiple Buildings on a Site**

The aerial photograph shows a site with street frontage on four sides. The development lines up the streets (commercial along one street, multifamily along two, and live-work units on the fourth side) and parking is located interior to the block/development

12. There are very few large sites along San Marco Avenue. Most of the larger sites are in the north area, where San Marco Avenue merges into US 1. In the case of large sites where there is adequate room to place buildings in the rear of the site (away from San Marco Avenue), the placement of such buildings in the rear is permitted as long as one or more buildings are built in front meeting the front yard setback requirements of the corridor (see *Figure 8 & 9* for an acceptable design alternative). The building(s) along San Marco Avenue shall be built first. Access drives shall be incorporated into the site layout to create connectivity to other sites and streets, and such drives shall be designed to resemble streets (pavement, sidewalks, markings and curbs). The main access drive to the site shall be centered on the anchor building (if allowed by driveway permits) and shall be lined with buildings or sidewalks and urban landscaping.
13. Sites with frontage on San Marco Avenue and US 1 shall give priority to the development standards applicable to the San Marco Avenue corridor. The setback along US 1 must also be deemed to be a “front” and shall meet the front yard setback of the applicable zoning district (see *Figure 9*).
14. Existing buildings on a site subject to redevelopment will not be required to meet the setback standards listed above. However, if technically feasible and not detrimental to the architecture of the existing building, any additions to the building shall serve to bring the building closer into compliance (see *Figure 10*).

## 4.2 Building Frontage

The purpose of the following building frontage requirements is to ensure façade continuity along San Marco Avenue and to avoid large voids between buildings. The building frontage standards are stated as a proportion of the building width within the required building setback relative to the width of the development site measured at the site frontage line. Portions of the building façade outside the required front/street building setback do not count as building frontage (see *Figure 11*).

1. The building frontage (a+b) shall be a minimum of 70% of the site frontage (C).
2. For redevelopment only, mid-century lodging developments are subject to a minimum 25% building frontage requirement (a+b) in order to allow the middle courtyard to remain. The courtyard, however, shall be screened with a street wall (see *Section 5*).
3. Sites with frontages on multiple streets shall be required to meet the minimum primary frontage along San Marco Avenue.
4. The canopy width of a *Significant Tree* along the site frontage may be counted towards the building frontage requirement as “building frontage” if tree canopy is not significantly altered.

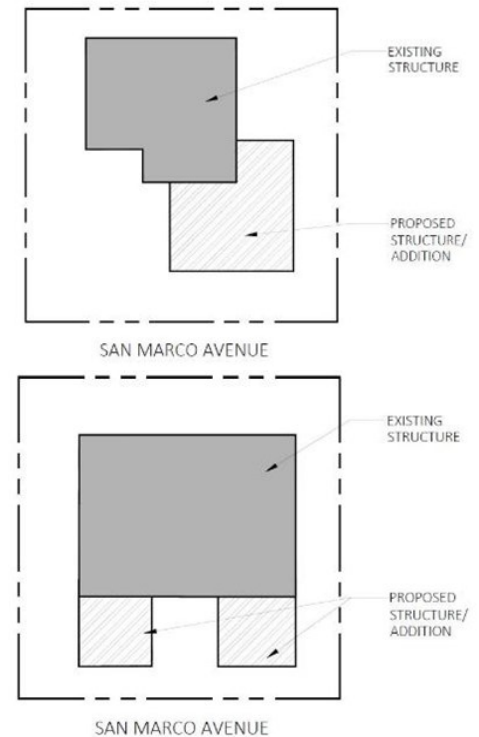


Figure 10. Building Additions  
Examples of building additions bringing the building closer to the street.

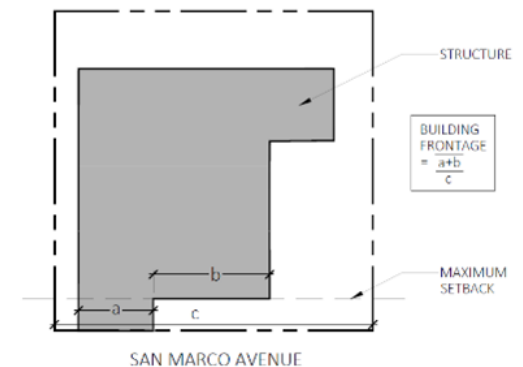


Figure 11. Measuring Building Frontage





## 4.3 Building Height

The maximum building height shall be per zoning.

1. Within the first 20', measured from the front property line, the maximum height shall be 25' measured from design flood elevation. This portion may be utilized for a roof top deck, but any awnings or coverings shall be a minimum of 10' from the front façade. Note that a rooftop deck shall not be placed above 27' anywhere on the building, and an 8' minimum solid sound barrier shall be placed on the rear face to block sound from moving toward adjacent residentially zoned sites.
2. There shall be a minimum height of fifteen (15) feet for the Commercial Typology buildings. This height will allow the provision of adequate size windows, awnings or canopies, signage and a decorative parapet to hide roof-top mechanical equipment. See *Figure 12*.
3. Residential Type buildings shall not exceed two and one half (2½) stories.
4. Unoccupied accessory towers, spires and cupolas may exceed the maximum height by a maximum of fifteen (15) feet. These features shall be in character with the architecture of the building and not designed solely as sign features or aesthetic value.
5. Commercial Type buildings shall have an interior first floor vertical clearance of at least twelve (12) feet to allow adequate exterior street level scale and adequate room for overhead retail utilities in ground floor spaces.



*Prohibited*



*Permitted*

**Figure 12: Minimum Building Height**



## 4.4 Architecture

The San Marco Avenue area is known for a diversity of architecture and building typology. Most buildings currently feature historic styles such as Frame and Masonry Vernacular, Traditional Commercial, and a few Georgian Revival, Modern, Mission Revival and Bungalow. There is a stronger historic character in the south portion of the corridor, serving as an extension of the historic downtown architecture. Protecting the diverse architectural character of the area is a goal of these design standards.

Building design shall be addressed based on the development typology identified in Section 8. Specific architectural styles are not required, but new development shall respect the scale and orientation of the existing development along the corridor.

### 4.4.1 New Construction

New construction and renovations should focus on the predominant styles currently found in the corridor or on the subject building for redevelopment. Frame Vernacular for the Residential Typology, and Traditional Commercial for the Commercial Typology. Contemporary architectural design, not to be confused with prefabricated/mass produced architecture, will also be permitted in conjunction with the Commercial Typology, as long as the design respects the scale and character of the preferred styles. Existing Institutional developments shall use their existing architectural style or traditional commercial on any future additions and modifications on the site. Appendix A lists the elements characteristic of the styles found in the corridor, which should be preserved or used as inspiration when designing new buildings or renovations. The following section contains additional building design standards that are applicable to all development regardless of the building typology or architectural style.

### 4.4.2 Building Design: Typologies

#### A. Residential Typology

- i. New buildings using the Residential Typology should utilize the Frame (Florida) Vernacular architectural style (see Appendix A).
- ii. All structures on a site shall be of the same architectural character (Florida vernacular architecture found throughout St. Augustine).
- iii. Architectural details and materials shall remain consistent on all elevations but may be simplified on the interior side and rear facades.
- iv. Building volumes, regardless of the use of the site, shall have a residential scale.
- v. Each main building shall have a full or partial width front porch (open or enclosed).
- vi. Porch depth shall be no less than six (6) feet and no greater than twelve (12) feet
- vii. Porches shall extend for at least forty (40) percent of the façade width.



City of St. Augustine  
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- viii. The main roof shall be sloped (side gable or hip). The porch roof may be hipped, shed, gable or incised.
- ix. Roof dormers are encouraged and shall be no wider than the width of the window plus corner casing.
- x. Dormer windows must be consistent with all the other windows. Painted glass shall not be allowed.
- xi. The street side façade shall be undulated – recesses or projections will be required every thirty (30) horizontal feet of the façade.
- xii. Windows shall have a vertical orientation and may be single or double hung. Paned windows are encouraged but not required.
- xiii. Frosted or patterned glass is not permitted on windows facing the street. Stained glass, however, is permitted.
- xiv. Shutters, if used, shall be one half the width of the window and shall be (or appear to be) operable.

## **B. Commercial Typology**

Buildings using the Commercial Typology shall adopt the Traditional Commercial architectural style if located south of Picolata Road (see *Appendix A*). Developments north of Picolata Road are allowed to use the Traditional Commercial or a modern/contemporary style (not to be confused with prefabricated/mass produced architecture). Regardless of the architectural style, all development using the Commercial Typology must meet the following standards:

- i. Large building volumes shall be divided to appear as smaller volumes grouped together. Volume breaks may be achieved by building wall projections and recesses, and varying heights and roof lines. Therefore, building facades shall not exceed fifty (50) feet along a street frontage without providing a substantial volume break such as a recess, or an architecturally prominent public entrance (Figure 12). The recesses and projections shall have a minimum depth of two (2) feet.
- ii. Façades shall not exceed twenty (20) horizontal feet and ten (10) vertical feet without including at least one (1) of the following elements (see *Figure 14*):
  - A window or door
  - Awning, canopy or marquee.
  - An offset, column, reveal, void, projecting rib, band, cornice, or similar element with a minimum depth of six (6) inches.
  - Arcade, gallery or stoop.
  - Complementary changes in materials or texture.
- iii. Architectural treatments on the façade, such as parapets, cornices or expression lines, shall be continued around the sides of the building.

## C. Existing Buildings/Renovations and Additions

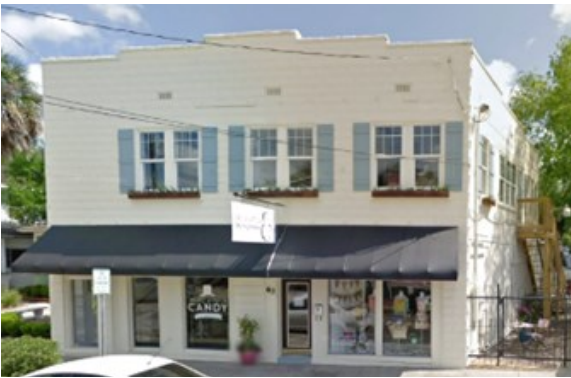
Existing buildings that are being renovated or expanded shall meet the following standards:

- i. Buildings that already feature a particular style or present some of the characteristics of a style shall be carefully restored to preserve such features. Otherwise, they should be redesigned to improve compatibility with the context of San Marco Avenue. Where a building exists that is considered a legacy building, redevelopment shall maintain the character features of the building and site arrangement. Primary features such as window shapes and sizes, opening patterns and entry doors, roof shapes and material pattern, siding patterns and finishes, porch and stair elements, foundations, and storefronts may be repaired and altered to the extent that the overall character of these elements is maintained.
- ii. Additions shall be designed so that there is the least possible loss or obstruction of character-defining features. The dominant features and facades of the existing building shall continue to be the prominent visual feature of the streetscape such that the new addition will not block or dwarf the existing building.
- iii. For rehabilitations of existing buildings, historic features used for architectural detail shall be preserved, restored, or reconstructed unless technically or economically infeasible, unless the removal is approved by the CRC. Removal will only be permissible if deemed necessary by the CRC. The Applicant shall note the historic and architecturally significant elements on the building elevations submitted, along with a statement noting if the element will be retained, removed, or repurposed.
- iv. If the site of a building that has been identified as using the Suburban or Auto-Service typology is being substantially redeveloped (defined in *Section 8*), it shall meet the standards for the Commercial Typology



Figure 13. Building Massing Example





*Allowed: Top: Brick counts as one of the three colors;  
Bottom: Door and awnings provide accent colors*



*Figure 14. Color Highlighting Architectural Features*

*Prohibited: Monochromatic schemes are not allowed*

### 4.4.3 Building Color

- A **minimum combination of three (3) colors and maximum of four (4) colors** shall be required per Commercial Type building: one for the main body, another for the trim and the third one for doors or other architectural features. **A minimum of two (2) and a maximum of four (4) colors** are required for Residential Type buildings. **Natural materials** such as brick, aluminum, stone, and hardwoods **count towards the minimum/maximum**. See Appendix C for color palette.
- Bands of color** around the building are not allowed, unless they are consistent with the pre-approved colors and **highlight an architectural feature**.
- An applicant that meets the statutory criteria of Section 553.79(20)(a), Florida Statutes, **must clearly identify** in the application materials the specific elements of the proposed project that apply to the statute. Once identified, these statutory design elements will be approved administratively.
- If an applicant wishes to use a non-pre-approved building color, the applicant **may submit** the color(s) for review and approval by the CRC. If noted as such, **approved colors shall apply to all future projects** and shall amend the approved colors noted in Appendix C.
- Building elevations (secondary/interior side façades) shall have the same color and materials as the façade facing the street.

### 4.4.4 Building Materials

Allowable exterior finishes shall be selected based on the architectural style (see Appendix A). Exposed pre-engineered metal buildings, metal siding that is found on pre-engineered metal buildings, plastic panels, and other veneers are not allowed, unless a Modification of Standard is approved by the CRC. Unpainted wood siding is not allowed; however, natural siding (cedar, cypress, ipe, and natural hardwoods) may be left unpainted.



#### 4.4.5 Fenestration and Glazing

Fenestration is the arrangement of windows and doors on a façade while glazing is the proportion of glass to the solid surface of a façade. Fenestration shall be calculated as the total area of glass (windows and glass doors), or openings in the case of open porches, divided by the façade area measured from design flood elevation to eight (8) feet above ground in the case of the first story, and from the floor of the upper story to eight (8) feet above that line for upper stories (see Figure 15). If the façade includes several planes, they all count toward the calculation if the façade planes are within the required building setback.

- For the Commercial Typology, the percent of fenestration shall be 30% minimum and 65% maximum of the ground floor facade facing the street. The percent of fenestration on upper floors facing the street shall be 20% minimum and 40% maximum.
- For the Residential Typology, the percent of fenestration shall be 20% minimum and 50% maximum for all floors facing the street.
- The size and orientation of windows shall reflect the architectural style of the building (see Appendix A).
- Glass shall be clear float (thin, smooth, clear glass surface), gray light, or solar glazing with 80% minimum transmittance. Reflective glass or film is not allowed.
- Glass panes shall not exceed a maximum size of 6 feet by 8 feet.
- Windows shall not be obscured or visually blocked by paint, shelves or attachments, but blinds and curtains are allowed to achieve privacy.
- Bulkheads shall extend a minimum of 12 inches and a maximum of 30 inches above the finished floor (see Figure 17). Existing garage and bay doors reused as windows are exempt from meeting this requirement. Buildings that are required to be elevated due to floodplain regulations may measure these distances from the interior finished floor.

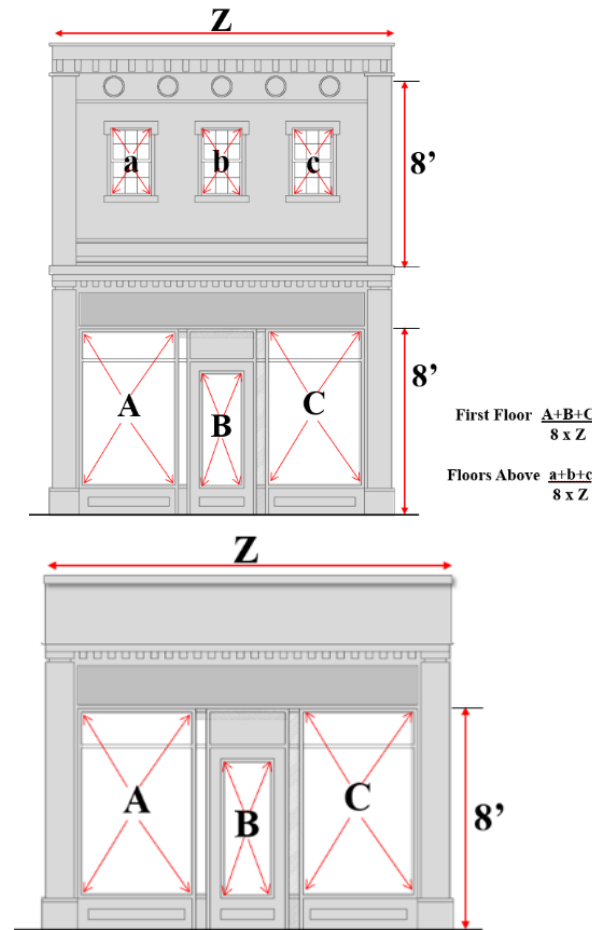


Figure 15. Calculating Glazing



See Section 4.4.5a & b



See Section 4.4.5a & b

Figure 16. Examples of Fenestration

#### 4.4.6 Awnings and Canopies

- Awnings or canopies (see *Figures 18, 19 & 20*), if installed, shall be placed between the first and second stories to provide protection from the weather and to delineate the transition between ground and upper floors.
- Awnings and canopies shall not obscure prominent architectural features, such as cornice lines, parapets, decorative window trim, etc. Stretched awnings on curved aluminum frames are not allowed.
- Awnings shall be placed in a way to **accentuate the architectural features** of the building. Therefore, awnings shall not be installed to extend over multiple windows and doors. Figure 20 shows two examples, one where the awning was placed entirely across the façade (prohibited), while the other example shows the awnings used to accentuate the windows (allowed).

#### 4.4.7 Building Entrances

- The main entrance of all buildings shall be oriented toward San Marco Avenue.** The design of the façade shall reinforce the presence of the main entrance.
- The front entrance (facing San Marco Avenue) shall be operable, clearly-defined and highly visible. To emphasize entrances, they shall be accented by a change in materials around the door, recessed into the façade (alcove), or accented by an overhang, awning, canopy or similar feature.
- Doors may be solid or glazed. The acceptable door materials include wood, painted steel, and painted aluminum.
- Pedestrian connections** from the public sidewalk and parking areas to the building entrance **shall be provided**. Buildings with entrances at a higher elevation than the public sidewalk shall create a transition within the setback (see *Figure 21*).



#### 4.4.8 Building Roof

- a. Buildings with **flat roofs shall have a cornice treatment or a parapet**. The cornice shall be at least eighteen (18) inches in height. Parapet shall be a minimum of two (2) feet in height measured from the exterior horizontal roof surface.
- b. The vertical dimension of a hip roof shall not exceed the height of the supporting walls.
- c. In the case of flat roofs, roof materials shall be light-colored or a planted surface (green roof).
- d. **Mansard roofs shall be prohibited** (see *Figure 22*).

#### 4.4.9 Parking Structure Design

- a. **Stand-alone parking garages, where allowed by the zoning district, shall require approval by the CRC.**
- b. Parking structures accessory to a principal use are required to be **placed behind a liner building** that houses active uses (e.g. commercial, office, residential). The liner building, which may be attached or detached from the parking structure, may be one story but shall extend for a minimum of seventy-five (75) percent of the length of the parking structure, and shall have a minimum depth of thirty (30) feet (see *Figure 23*).
- c. Any portion of the parking structure that is not concealed behind a building shall be screened to conceal all internal elements such as plumbing pipes, fans, ducts and lighting. Ramping should be internalized wherever possible. Exposed spandrels shall be prohibited.
- d. The portion of the parking garage not concealed behind the liner building shall provide a landscaping strip along the façade. The landscaping strip shall have a minimum depth of 5 feet.
- e. The **exterior facades** of the parking garage shall be designed to **achieve architectural unity** with the principal structure.
- f. Parking structures shall meet setback, height, façade articulation, fenestration and glazing standards contained in this section. See *Section 4.4.5* for details on fenestration and glazing.
- g. Ventilation openings shall resemble typical fenestration found within a particular area.



Figure 17. Bulkheads



Figure 18. Example of Awning



Figure 19. Example of Canopies





## 4.5 Site Access and Parking

### 4.5.1 Pedestrian Access and Circulation

Pedestrian walkways, ramps or steps (or a combination of these) shall link the public sidewalk to the front door entry.

### 4.5.2 Vehicular Access

Parking areas shall be accessed from a secondary street, or from an adjacent property (shared use agreement necessary), if any of these are available or proposed as part of the development. Driveway connections to San Marco Avenue shall be allowed as a last resort and shall require the applicant to provide evidence to this fact. The following standards shall apply to all driveways:

- Only one driveway (ingress and egress) shall be allowed per site. A one-way ingress and one-way egress shall equal one driveway. Only if required by the Fire Marshal for safety, additional driveway(s) may be permissible.
- One-way drives shall not exceed 13 feet in width.
- Two-way driveways shall not exceed 24 feet in width.
- Turning radii along San Marco Avenue will be determined by the applicable authority in the right of way.
- Turning radii along all other streets shall not exceed 20 feet.

### 4.5.3 Parking Placement

Surface parking lots shall be located **behind the building** (see *Figure 24*). However, the following parking placement alternatives are also allowed:

- Where a site is too shallow to provide parking behind the building, the placement of parking facilities is permitted on the side of the proposed building. This is allowed only if the building setback and building frontage requirements are met. A street wall, in addition to landscaping, will be required to screen the parking facility.
- The placement of parking facilities in front of a building is only permitted for the portion of



*Prohibited (across several windows)*



*Allowed (individual awnings for each window)*

*Figure 20. Awning Placement*



*Figure 21: Pedestrian Transition*

the building that is not used to meet the building setback and building frontage requirements. A street wall, in addition to landscaping, will be required to screen the parking facility.

#### 4.5.4 Existing Non-Conforming Parking Facilities

There are several sites in the district with parking spaces that do not meet current code standards in terms of access, location, or size.

- Parking in Front of the Building.** In conjunction with the redevelopment of a site, parking facilities that are already located in front of a building that is not being moved or demolished (*Figure 25*) may remain.
- Parking Spaces Accessed from Street.** In conjunction with the redevelopment of a site, existing parking spaces that are accessed directly from the street (as opposed to a parking lane) may remain as long as there is no option to provide the minimum parking requirements elsewhere on the site.
- Non-Conforming Curb Cuts and Driveways.** Will only be required to meet the standards of this section if there is substantial redevelopment on the site.

#### 4.5.5 Bicycle Parking

In conjunction with new construction and substantial redevelopment (as defined in Section 2), the applicant may provide bicycle parking facilities on the site at a suggested minimum rate of one bike rack space per 20 required vehicle spaces. The rack may be placed within the street setback area, but not within the public right-of-way, and it shall count towards the required front or side yard buffer (2' of width per bicycle space, not to exceed 6') or as an offset to required trees at a rate of one tree per three spaces (not to exceed two (2) trees).



Figure 22. Examples of Mansard Roofs

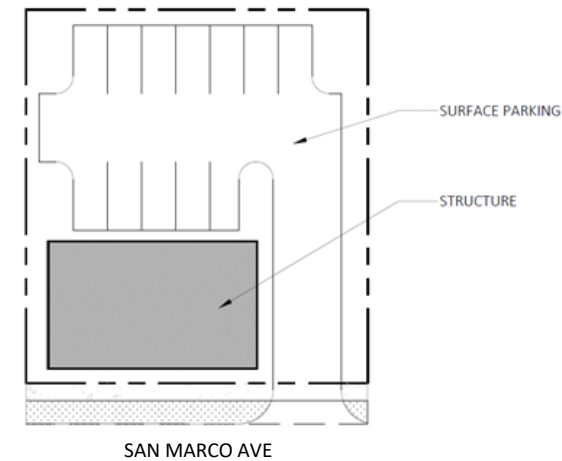


Figure 24. Parking Location and Access



Figure 23. Parking Structure with Liner Building



Figure 25: Examples of Existing Non-Conforming Parking Facilities

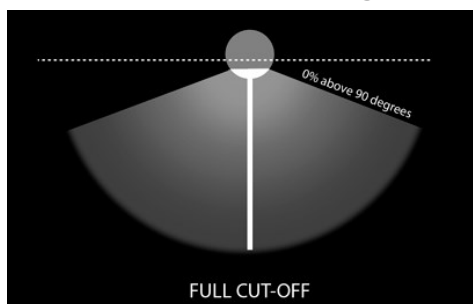


Figure 26. Full Cut-Off Lighting

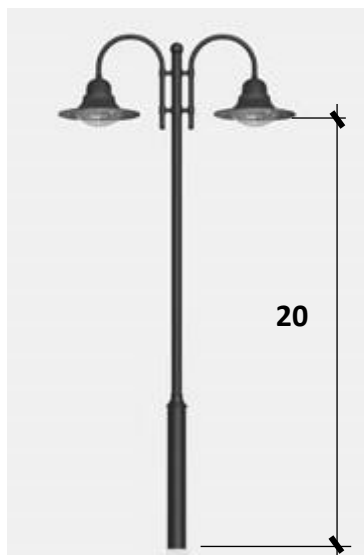


Figure 26b. Lighting Height

## 4.6 Site and Building Lighting

Acceptable lighting on the site may include lights on the exterior of a building, up or down lighting shining on buildings or trees, and parking lot pole lighting. Only product information sheets shall be required for submittal.

### 4.6.1 Architectural Lighting

- Lighting fixtures on the building shall be compatible with the architecture of the building and measure between 2700-3000K.
- Permanent accent lighting designed, distinct from seasonal or holiday lighting, to accentuate building details is encouraged.
- Holiday lighting, such as for Nights of Lights, may be additionally regulated by the City Commission.
- Color and/or neon** may be used as architectural lighting to accentuate building frame, profiles or details, with **Corridor Review Committee approval**.

### 4.6.2 Site + Parking Lighting

- For basic site lighting applications (such as parking lot, pathway, and area lighting), **full cut-off lighting** fixtures shall be used for all exterior lighting. Such lights shall not point directly at the right-of-way nor the sky (see Figure 26). **Source of illumination shall not be visible from a residential district.**
- Exposed interior and exterior fluorescent or incandescent bulbs that are visible from the public right-of-way are not allowed.
- Lighting poles may not surpass 20' in height as measured to the point of light output (see Figure 26b).
- LED dimming technology shall be incorporated in all exterior lighting mounted higher than 10'-0" to reduce lighting level to minimum safe illumination after business closing hours, if possible. Refer to Illuminating Engineering Society guidelines. Minimum for a private parking lot is 0.13 foot-candles. A Lighting professional shall confirm minimum lighting level.

## 4.7 Stormwater Facilities

**Traditional stormwater retention facilities** (such as ponds), where required, **shall not be located within the front setback**. Stormwater is recommended to be incorporated into the overall design of the project as amenities (The goal of encouraging the use of these mechanisms is to reduce stormwater runoff, capture contaminants closer to the source and reduce the use of potable water for irrigation and grey water activities). Minimum criteria are below.

Projects shall include **at least two of the following** low impact design concepts, which may be located anywhere on the project (including the front setback):

- **Rain water harvesting** (rain barrels, underground cisterns, and similar)
- **Bio-swales**
- **Rain gardens**
- **Green roof** (layer of vegetation planted over a waterproofing system that is installed on top of a flat or slightly-sloped roof)
- **Pervious pavement** (pervious concrete, pervious pavers, and/or other pervious pavements recognized by the United States Green Building Council). To be used for 10% minimum non-vehicular and vehicular parking for credit.



*Figure 27. Examples of Stormwater Facility Design*

*Sources: The Urban Report; Green and Sustainable Services, LLC.*









## Section 5

# LANDSCAPE & SCREENING



## SECTION 5. LANDSCAPE & SCREENING

### 5.1 Landscape Introduction

This section contains the detailed landscaping standards for properties along San Marco Avenue. The quality, installation, mitigation, and maintenance requirements of the City Code shall apply (see Section 25-53 of the Code). Note: the landscape requirements for Residential (5.4) and Commercial (5.5) typologies are separated in this document.

#### 5.1.1 Plant Materials

In general, site landscaping shall consist of **native** or **naturalized plants** that thrive in the St. Augustine climate and environment (see plant list in Appendix C). Plants shall be relatively drought tolerant to encourage the conservation of water.

#### 5.1.2 Protection of Existing Landscape

Effort shall be made to minimize disturbance of terrain around buildings or elsewhere on the site, thus reducing the possibility of destroying or damaging important landscape features or archaeological resources. Construction plans shall **indicate areas of protection** and shall have **applicable notes regarding minimal disturbance of landscape elements to be preserved**.

Along the corridor, existing oaks in good health shall be protected when possible. **Oaks over 12" dbh** to be removed shall have **an additional 25% deficit applied to current tree mitigation calculations**.

- Note: A *Protected Tree* along the site frontage can be included in the building frontage calculation.
- Note: 5.2-5.5 is divided into the Residential Typology and Commercial/Institutional Typology

### 5.2 Landscape Requirements: Residential

#### 5.2.1 Introduction

The residential typology is characterized by a large front lawn (fronting San Marco Avenue) along with large shade trees for canopy. Landscape reinforces the main entry point and other architectural features.



## 5.3 Front Yard Landscape: Residential

### 5.3.1 Landscape Buffer Yard Special Conditions

The front yard shall be defined as the entire area between the front façade of the building and the property line. Note that required trees and shrubs may be placed anywhere within this zone (versus the limitation of 10' from property line) so that trees and shrubs can accentuate entry points and foundation plantings appropriate for a residential typology.

### 5.3.2 Landscape Buffer Yard Classification

CM-2 zoned front yards, fronting San Marco Avenue as well as US 1 North (inside the San Marco Avenue Entry Corridor), shall have buffer yard as follows:

<b>Trees</b>	<b>Shrubs</b>
5 trees per 100' of frontage	25 shrubs per 100' of frontage

CL-2 zoned front yards, fronting San Marco Avenue shall have buffer yard as follows:

<b>Trees</b>	<b>Shrubs</b>
4 trees per 100' of frontage	20 shrubs per 100' of frontage

### 5.3.3 Percentage of Landscape

A minimum of 40% of the buffer yard length, not including drives, mechanical equipment, etc., shall be landscape. Raised planters are encouraged and shall count toward the buffer yard length; however, movable, potted, plants do not count toward the landscape buffer.



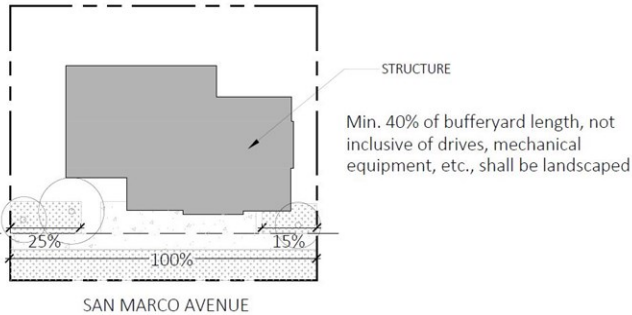


Figure 28. Landscape Percentage

## 5.4 Side Yard Landscape: Residential

### 5.4.1 Landscape Buffer Yard Classification

- a. For both CM-2 and CL-2, yards shall have buffer yard as follows:

Trees	Shrubs
4 trees per 100' of frontage	12 shrubs per 100' of frontage

- b. Screening buffers shall not be required between adjacent developed non-residential uses along San Marco Avenue; however, corner lots must provide landscaping on all street frontages.

### 5.4.2 Percentage of Landscape

A minimum of 70% of the buffer yard length, not including drives, mechanical equipment, etc., shall be landscape. Modification may be allowed if technically unfeasible.

## 5.5 Rear Landscape and Alleyway Buffer Yard: Residential

### 5.5.1 Landscape Buffer Yard Classification

- a. Rear yard for both traditional commercial and converted residential typologies shall have buffer yard as follows:

Trees	Shrubs
4 trees per 100' of frontage	12 shrubs per 100' of frontage

- i. When the rear yard setback is adjacent to a residential use: in addition to the 15 foot setback required in Section 4 and the landscaping required in Section 5, a 6-foot minimum (8-foot maximum) finished masonry wall (stucco, decorative segmental wall, brick) shall be constructed within the rear buffer yard of the site. The adjacent residential property owners may request directly from the applicant a wooden fence or landscape-only buffer and such agreement shall be filed with the building permit application with a notarized letter.



Figure 29. Examples of Planters



- ii. If the site use changes, the adjacent residents shall be contacted by the property owner to inquire if a wall is desired by the residents (if a wall was not already constructed).
- b. When any setback is adjacent to an alleyway with commercial zoning on both sides: the buffer yard classification shall be as follows:

Trees	Shrubs
3 trees per 100' of frontage	12 shrubs per 100' of frontage

### 5.5.2 Percentage of Landscape

A minimum of 70% of the buffer yard length, not including drives, mechanical equipment, etc., shall be landscape.

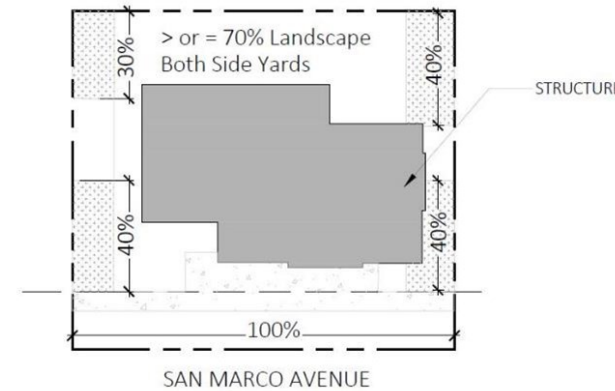


Figure 30. Side Yard Percentage: Residential

## 5.2 Landscape Requirements: Commercial/Institutional

### 5.2.1 Introduction

The commercial typologies are characterized by limited landscape in the front. Landscape is often in raised planters and narrow strips when provided.

## 5.3 Front Yard Landscape: Commercial/Institutional

### 5.3.1 Landscape Buffer Yard Special Conditions

For commercial typologies, up to 50% of required landscape may be shifted to adjacent side yard.



### 5.3.2 Landscape Buffer Yard Classification

CM-2 zoned front yards, fronting San Marco Avenue, shall have buffer yard as follows, \*except for additional required buffers.

<b>Trees</b>	<b>Shrubs</b>
5 trees per 100' of frontage	25 shrubs per 100' of frontage

CL-2 zoned front yards, fronting San Marco Avenue, shall have buffer yard as follows:

<b>Trees</b>	<b>Shrubs</b>
4 trees per 100' of frontage	20 shrubs per 100' of frontage

### 5.3.3 Percentage of Landscape

A minimum of 25% of the buffer yard length, not including drives, mechanical equipment, etc., shall be landscape. Raised planters are encouraged and shall count toward the buffer yard length; however, movable (potted) plants do not count toward a landscape buffer yard.

## 5.4 Side Yard Landscape: Commercial/Institutional

### 5.4.1 Landscape Buffer Yard Classification

- Screening buffers shall not be required between adjacent non-residential uses along San Marco Avenue.
- When side yard setback is adjacent to a residential zoning:
  - Side yard shall have buffer yard as follows:

<b>Trees</b>	<b>Shrubs</b>
5 trees per 100' of frontage	20 shrubs per 100' of frontage

- When side yard setback is adjacent to a roadway: the side yard shall have buffer yard as follows:
- Up to 50% of required landscape may be shifted to a front, rear, or other side yard buffer.
- Screening buffers shall not be required between adjacent non-residential zoning along San Marco Avenue.





### 5.4.2 Percentage of Landscape

A minimum of 70% of the buffer yard length, not including drives, mechanical equipment, etc., shall be landscape.

## 5.5 Rear Landscape and Alleyway Buffer Yard: Commercial/Institutional

### 5.5.1 Landscape Buffer Yard Classification

- a. Rear yard shall have buffer yard for both traditional commercial and converted residential typologies as follows:

Trees	Shrubs
4 trees per 100' of frontage	12 shrubs per 100' of frontage

- i. When the rear yard setback is adjacent to a residential zoning: in addition to the 15 foot setback required in *Section 4* and the landscaping required in *Section 5*, a 6-foot minimum (8-foot maximum) finished masonry wall (stucco, decorative segmental wall, brick) shall be constructed within the rear buffer yard of the site. The adjacent residential property owners may request directly from the applicant a wooden fence or landscape-only buffer and such agreement shall be filed with the building permit application with a notarized letter.
  - ii. If the site use changes, the adjacent residents shall be contacted by the property owner to inquire if a wall is desired by the residents (if a wall was not already constructed).
- b. When any setback is adjacent to an alleyway with commercial zoning on both sides: the setback shall be as follows:

Trees	Shrubs
3 trees per 100' of frontage	4 shrubs per 100' of frontage

### 5.5.2 Percentage of Landscape

A minimum of 70% of the buffer yard length, not including drives, mechanical equipment, etc., shall be landscape.

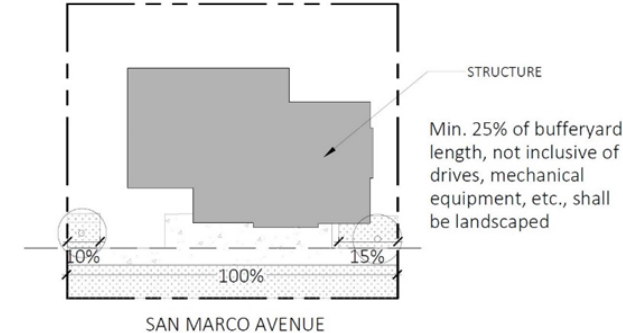


Figure 31. Landscape Percentage



## 5.6 Vehicular Use Areas

Vehicular use areas (VUAs) shall meet the following requirements in addition to the screening requirements set forth for the side and rear yards.

1. Within each parking area of five thousand (5,000) square feet or more, there shall be a minimum of five (5) percent of landscaped areas, which shall be reasonably distributed within each five thousand (5,000) square feet of parking area so as to be prorated to fulfill the objectives of this article.
2. Each separate planting area shall contain at least one (1) tree and a tree shall be planted for each one hundred (100) square feet of internal landscaping. All internal landscaping shall be protected from vehicular encroachment by curbing or wheel stops. All landscape areas shall be a minimum of one hundred (100) square feet, with the least dimension being five (5) feet.
3. The VUA perimeter landscape (shrub planting material) shall provide screening of vehicles, particularly on the rear and side yards. Reference **Appendix C** for hedge material recommended for parking screening.



*Figure 32. Examples of Street Walls*

## 5.7 Fences and Walls

While fences are typically used to achieve privacy on a site, street walls are used to continue the building frontage along a street, to screen vehicular areas such as parking lots, and to frame public zones such as courtyards and outdoor dining areas. Street walls and fences shall meet the following standards:

### 5.7.1 Fences

- Along the front property line and front setback, gates, railings, and fencing materials shall be painted wood, powder-coated aluminum, or wrought iron. Note that these elements are likely attached to the required site wall, as defined in section 5.7.2.
- Chain link, wire, and PVC fencing shall be prohibited in the front setback but may be used along side or rear areas.
- For the Residential Typology, fence may be used in lieu of a street wall as defined in 5.7.2

### 5.7.2 Street Walls

- In the **absence of a building façade along any part of the San Marco Avenue site frontage line**, a **street wall shall be built** in line with (co-planar with) the façade. Breaks are permitted in the street wall to provide vehicular and pedestrian access to the site, for plazas and gathering spaces, and for the purpose of tree protection. Street walls shall have openings no larger than necessary to allow automobile and pedestrian access.
- Street walls are the only types of fences/walls allowed facing streets** (see *Figure 32*). Privacy fences and walls are not allowed between adjacent uses along San Marco Avenue.
- Street walls shall be located within the front setback, and may have landscaping or hard surface between the wall and the public sidewalk.
- Street walls shall be a minimum of 3 feet in height and shall not exceed a 5 foot maximum, as measured from the lower elevation of the public sidewalk or final elevation of adjacent interior development. However, the portion of the wall above 3 feet shall be no more than 50% solid.
- Street walls shall be constructed of brick or masonry. Fencing, as per 5.7.1, may be included.
- The area in front of a street wall may include landscaping or hardscape if used to expand the sidewalk.
- When landscaping is provided between the wall and the sidewalk, the landscaping strip shall be a minimum of two (2) feet wide.
- A three (3) foot wide landscape strip, as measured from the wall, shall also be provided on the interior side of the street wall.

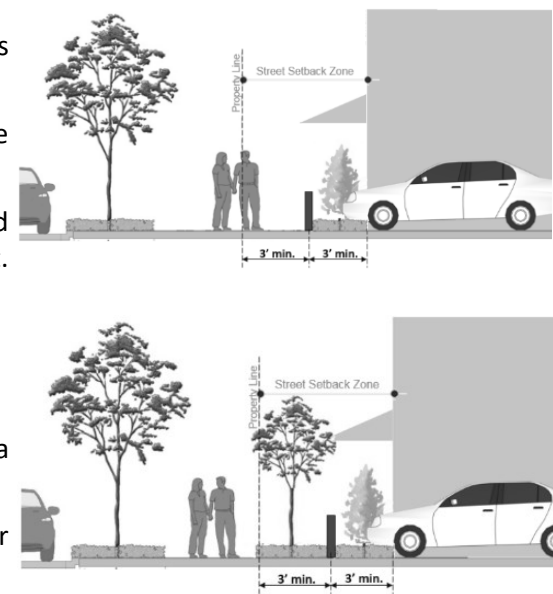
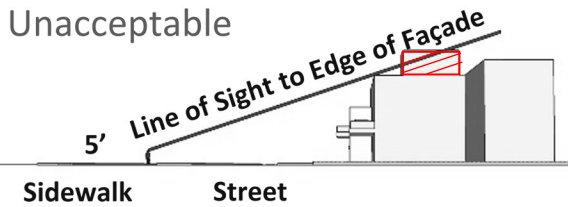


Figure 33. Street Walls and Landscaping





Unacceptable



Acceptable

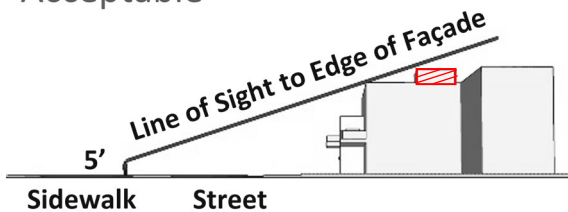


Figure 34. Equipment Screening

Screening rooftop equipment measured from furthest  
edge of right of way

Top: Unacceptable,

Bottom: Acceptable.

## 5.8 Mechanical Equipment

For the purpose of this section, mechanical equipment shall include, but is not limited to, a heating, ventilation, or air conditioning unit placed outside of a building; vehicular/parking equipment (except for gate arms, if applicable); and any utility-related equipment such as pumps, meters, transformers and similar equipment required to be installed on a development site. **Mechanical equipment** shall be **located so that noise and visual impacts upon abutting residential property are minimized.**

1. Mechanical equipment shall not be permitted within any required building setback unless the utility company determines this to be the only possible location.
2. Mechanical equipment shall not be located between the building and the street, unless technically infeasible. On corner lots, the location of mechanical equipment along a secondary street may be permitted through the modification of standards process (see *Section 3.7*) subject to the screening requirements stated below.
3. **Screening** (by wall, solid fence or vegetative matter) that **exceeds the height of the equipment** shall be **required if the equipment is visible from the street** or adjacent properties. Certain equipment, such as, but not limited to rain barrels or cisterns, shall not require screening provided they are designed to blend with the building and are constructed with the same materials and colors.
4. Air conditioning units and elevator shafts are permitted to go above the standard building height limit but shall be visually screened from all public rights-of-way as shown in Figure 34. This restriction may limit the number of stories that can be built within the 35 foot maximum building height.



## 5.9 Solid Waste Disposal Facilities

1. Solid waste disposal facilities, solid waste, recycling, yard trash containers, and grease containers must be located to the side or rear of the building and shall be concealed from view from all sides by a wall or fence of up to 7 feet in height. The enclosure shall be made of wood, masonry or vinyl and shall be fitted with an opaque sliding or hinged door and working latch. Gates shall be able to be secured in the open position. No chain link fence is allowed for solid waste enclosures.
2. On corner or double-frontage lots, the disposal facilities shall be accessed from the secondary frontage.
3. Enclosure shall adhere to City Solid Waste requirements.
  - a. Width of 10-12 feet minimum.
  - b. Depth of 10-12 feet minimum.
  - c. Height of 7 feet minimum.
  - d. Overhead clearance over and in front of dumpster shall be clear of obstructions 20' in height.
  - e. Bumper posts: two (2) steel, 4" diameter set in concrete. Placement 12" from back wall of enclosure.

## 5.10 Visibility Triangle

All landscaping within the triangular areas described below shall provide unobstructed cross visibility at a level between three (3) and eight (8) feet above the ground. Trees having limbs and foliage extending into this cross visibility area shall be allowed, provided that they are so located so as not to create a traffic hazard. Landscaping, except grass and ground cover, shall not be located closer than three (3) feet from the edge of any accessway pavement. The triangular areas are:

1. The areas of property on both sides of an accessway formed by the intersection of each side of the accessway, i.e. driveway, and the public right-of-way pavement line with two (2) sides of each triangle being ten (10) feet in length from the point of intersection and the third side being a line connecting the ends or the other sides.
2. The area of property located at a corner formed by the intersection of two (2) or more public rights-of-way with two (2) sides of the triangular areas being measured thirty (30) feet in length along the abutting edges of pavement, from their point of intersection, and the third being a line connecting the ends of the other two (2) lines.



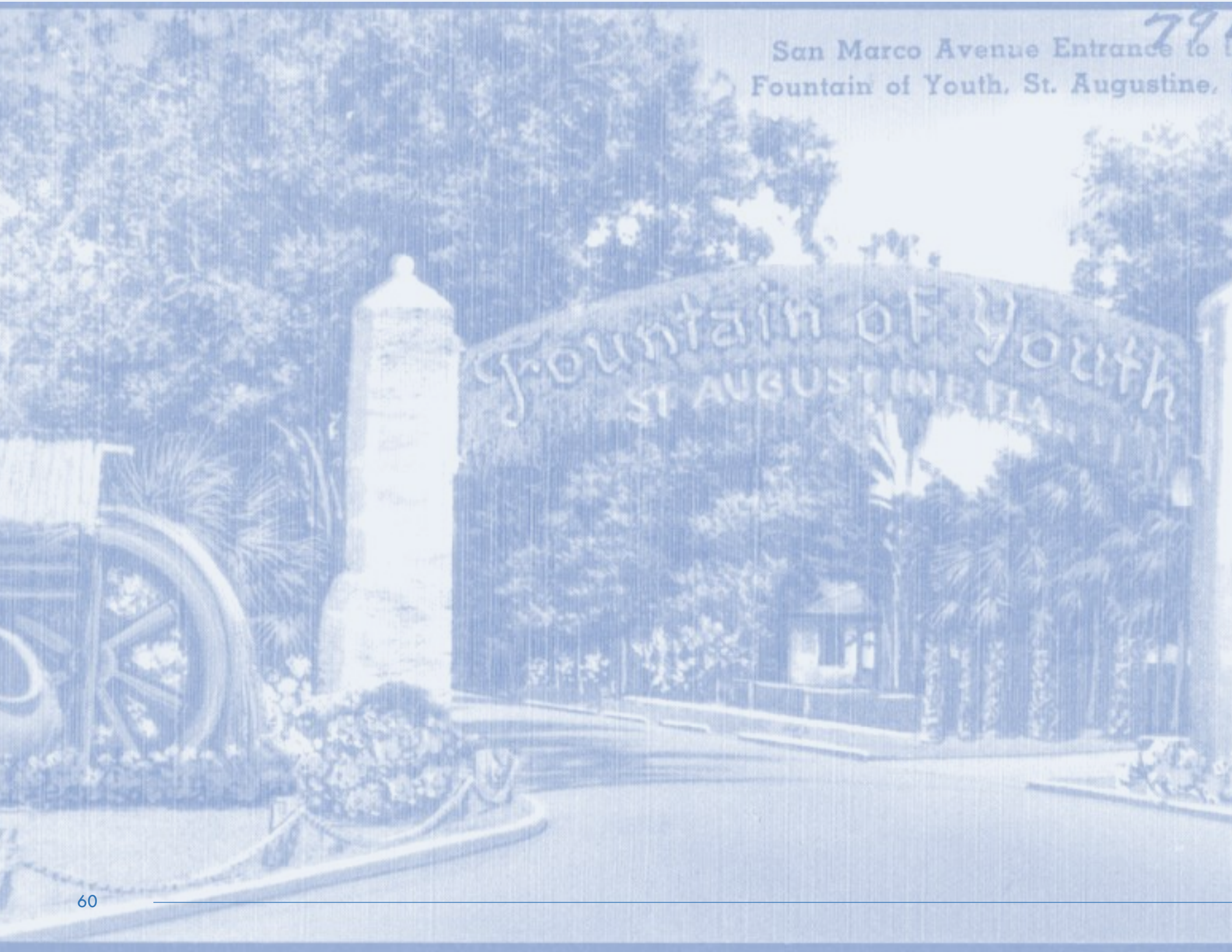
Fig- ure 35. Visibility Triangle





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79  
San Marco Avenue Entrance to  
Fountain of Youth, St. Augustine,





## Section 6

# SIGNS





## SECTION 6 SIGNS

The Corridor regulates sign types, attachment points on buildings, and illumination as noted below in Section 6. Existing signs not meeting the requirements of this section are subject to the non-conforming provisions of the Code of Ordinances (see Section 3-23(k)).

### 6.1 Number and Types of Signs Allowed

1. The number of business identification signs attached to the building per business frontage on a public street shall be per City code.
2. The types of signs allowed are as follows (see Section 7, Definitions):

#### Building Signs

- Wall Sign
- Canopy Sign
- Awning Sign
- Projecting Sign
- Hanging Sign
- Window Sign
- Freestanding (**only for residential conversions**)

3. The use of portable signs, streamers and pennants shall follow sign code.
4. Blinking lights, bulbs facing the road, and electronic message signs are not permitted.

### 6.2 Building Sign Size

The total area of all building signs may be distributed among the various building signs allowed. The maximum size of any **individual** building sign shall be per City code.



Figure 36. Permitted Sign Illumination

## 6.3 Sign Illumination

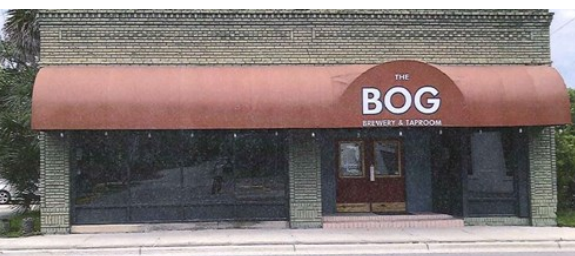
1. Interior-illuminated cabinet or channel letter signs are not allowed. Signs may only be illuminated from an exterior light fixture or backlit to accentuate opaque raised lettering or images.
2. There shall be no illuminated signs facing a single family residential use.



*Figure 37. Prohibited Sign Illumination*



*Figure 38. Canopy Signs*



*Figure 39. Awning Signs*

## 6.4 Building Signs

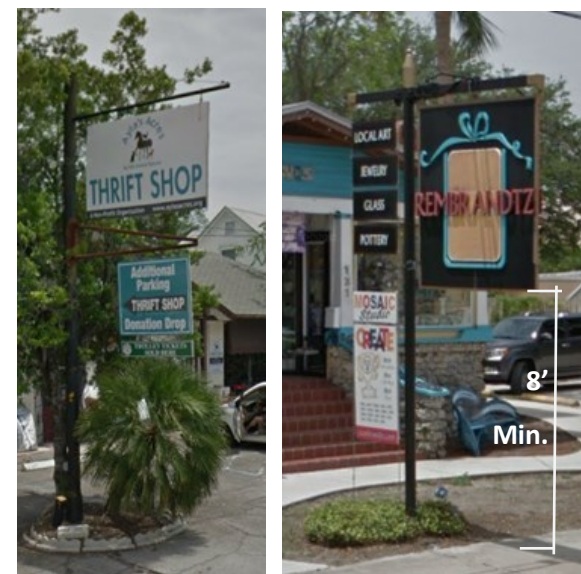
1. Wall signs shall be either a panel or individual letters applied to the wall, shall not project from the wall by more than 12 inches, shall not extend above the top of the wall where it is located, and in the case of two story buildings, it shall be placed between the first and second floor windows. Signs painted directly onto the façade are not allowed.
2. Projecting signs may be read horizontally or vertically, may extend into the building setback zone, but not over the public right-of-way. They shall have a clearance of 8 feet from the ground and shall not exceed the height of the building wall where installed.
3. Canopy signs may be applied to the face of the canopy and shall not exceed 50 percent of the width of the canopy. They can be placed fully or partially above or below the edge of the canopy, provided the sign consists of individual letters (as opposed to a panel). Canopy signs are also allowed to hang from the bottom of the canopy facing the street. An 8-foot clearance shall be provided.
4. Awning signs shall not cover more than 30% of the vertical face (or near vertical part) of the awning.
5. Window signs (silk screen, vinyl letters, gold leaf, hand painted or neon) shall not occupy more than 25% of the glass window or door, and may be allowed for first and second story businesses.
6. Hanging signs shall not count toward the maximum sign area allowed, provided it is placed under the awning or canopy, perpendicular to the building, and near the front door of the business. Such signs shall have an 8 foot minimum vertical clearance as measured from grade to the bottom of the sign, and a maximum height of 2 feet. While hanging signs do not count towards the maximum sign area, they do count toward the maximum allowed signs per business.
7. Signs shall not obscure architectural details such as windows, cornice, decorative brickwork and storefronts.

## 6.5 Free Standing Signs (only for Residential Conversions)

The need for freestanding signs is based on the placement of the building away from the street. When buildings cannot be seen by passing motorists, they need a larger sign by the street to indicate the location of the business. As future traditional commercial development in the San Marco Avenue corridor is expected to bring the buildings closer to the street, it will not be necessary to utilize freestanding signs to locate the businesses. The building itself, and the signs on the building, will be visible to the passing motorists.

The *Residential Typology*, on the other hand, contain buildings at a certain distance from the street. As an incentive for preserving this form of development, freestanding *pedestrian pole signs* will be allowed provided they meet the following standards:

1. The sign shall not exceed a height of fifteen (15) feet measured from the adjacent sidewalk and shall provide a clearance of 8 feet from the ground if it overhangs a pedestrian or vehicular circulation area.
2. Pedestrian pole signs may be supported by one or two poles, or may be suspended from the structure as shown in Figure 40. The supporting poles may not exceed a width of 6 inches (each).
3. The sign shall not be placed within 5 feet from any portion of the building.
4. The sign may be placed up to the front property line but **shall not extend over the public right-of-way.**



**Figure 40.** Examples of Pedestrian Pole Signs

(Note that all new signs shall not extend over the public sidewalk)







## Section 7

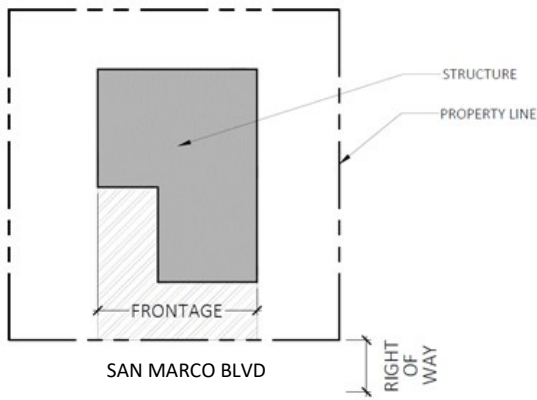
# DEFINITIONS



*Awning*



*Canopy*



*Frontage Diagram*

## SECTION 7. DEFINITIONS

**Accessory:** a supplementary material object or feature that enhances the main focus

**Adjacent:** next to or adjoining something else.

**Ancillary:** a subsidiary area that supplements and is integral to the functions of the primary area, but is in no way the foremost purpose of the building

**Appurtenances:** spires, belfries, cupolas, antennas, water tanks, ventilators, chimneys, elevator shafts or other appurtenances required to be placed above the roof level

**Arcade:** series of arches supported by a colonnade of vertical supports, such as columns or piers

**Awning:** a rectangular or curved shelter extending from a building and supported entirely from the exterior wall of a building and composed of non-rigid materials except for supporting framework.

**Canopy:** a structure constructed of rigid materials including, but not limited to metal, wood, concrete, plastic or glass, which is attached to and supported by a building or by columns, poles or braces extended to the ground.

**Corridor Review Committee:** development review board as designated by the City Commission

**Design Flood Elevation (DFE):** The elevation of the highest flood (generally the Base Flood Elevation including freeboard) that a retrofitting method is designed to protect against.

**Development Site:** a parcel or lot of land that has been designated and initiated work for any development projects

**Dimensional Requirement:** the code guidelines for numerical measurement conditions for which development has to abide by, such as setbacks, structure widths & heights, lot coverage area, etc.

**Facade:** the face of a building, especially the principal front that looks onto a street or open space

**Facility:** a structure, amenity, or place that houses and supports a specific function

**Frontage:** the facade of a building, specifically abutting King Street.

**Full Cut Off:** a lighting fixture from which none of its lamp's intensity is emitted at or above a horizontal plane drawn through the bottom of the fixture and no more than ten percent of the lamp's intensity is emitted between eighty (80) degrees and that horizontal plane.





**Land Development Regulations:** the land development and zoning codes contained in the City of St. Augustine Code of the Ordinances.

**Licensed Professional:** a person who has been granted legal authority by their governing body to practice within the designated scope of their profession

**San Marco Avenue:** the main vehicular corridor that front setbacks relate to

**Non-dimensional:** the code guidelines for non-numerical measurement conditions for which development has to abide by, such as landscape and building color

**One-Story:** having one floor or level

**Parking Structure:** a building, generally enclosed, of which the principle function is for vehicular parking

**Parapet:** a low wall or protective barrier projecting from the edge of a building roof, balcony, terrace, or similar structure

**Permanent Accent Lighting:** light fixtures such as uplights, downlights, and strip lighting designed to accentuate architectural or landscape features (holiday / Christmas lighting not included)

**Photometric:** a plan showing the simulated lighting coverage of a proposed development, helping designers assess if a plan will be sufficient in lighting a space while not creating a lighting disturbance to the surrounding properties

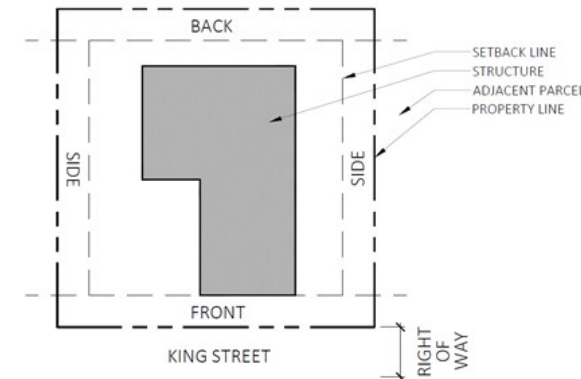
**Porte-Cochere:** a covered entrance large enough for vehicles to pass through

**Rendering:** a graphic (developed by a licensed professional) depicting a proposed structure in an existing context

**Redevelopment:** construction, policies, or programs with the overall process of improving upon existing sites or structures

**Right of Way (R.O.W.):** publicly owned lands, typically with roadway, sidewalks, etc., which is often immediately adjacent to a property line.

**Seasonal or Holiday Lighting:** a series of clear, white or colored lights, either bare or shielded within a enclosure.



*Setback Diagram*



*Sign, Awning*



*Sign, Canopy*





City of St. Augustine  
San Marco Avenue Design Standards



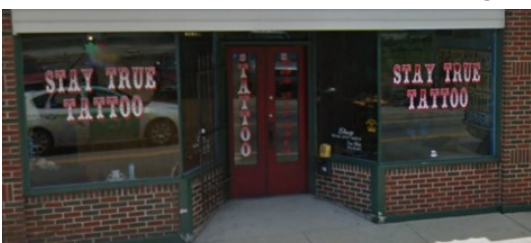
*Sign, Hanging*



*Sign, Projecting*



*Sign, Wall*



*Sign, Window*

**Setback:** means the mean horizontal distance between the front line of the building (excluding steps and unenclosed porches of not more than sixteen (16) square feet) and the front line of the lot.

**Sign, Awning:** a sign that is painted, silk-screened, stitched on, imprinted on, or otherwise applied directly onto the fabric of the awning.

**Sign, Canopy:** a sign which is suspended from, attached to, supported from or forms a part of a canopy.

**Sign, Freestanding:** a sign supported by a sign structure secured in the ground and which is wholly independent of any building, fence, vehicle or object other than the sign structure for support. This sign type is prohibited on King Street (see Pedestrian Pole Sign for residential typology).

**Sign, Hanging:** a sign that is placed under a canopy, awning or arcade, perpendicular to the building façade, and not intended to be seen by motorists.

**Sign, Pedestrian Pole Sign:** a sign supported by one or two poles in the ground fully independent of any building, fence, vehicle, or object, not to exceed the dimensions included in the King Street Design Standards, where applicable.

**Sign, Projecting:** a sign which is affixed and displayed perpendicular to any building wall or structure with a bracket

**Sign, Wall** – a sign that is attached flat to a building wall.

**Sign, Window** – a permanent sign affixed to, or painted on either face of a window or glass door that leads to the exterior of the building. Signs suspended within three (3) feet behind the glass are also deemed window signs.

**Substantial Redevelopment:** site and building improvements not subject to the building code (design/aesthetic improvements) that exceed 50% of the building value over a five year period will be deemed a substantial improvement for the purposes of the Design Standards and subject to review for compliance.

**Two-Story:** having two floors or levels

**Vehicular Use Area:** all paved surfaces connected to or associated with an off-street parking area included but not limited to access driveways, aisles, curbs, islands, ramps, and vehicle parking spaces



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## Section 8

# DEVELOPMENT TYPOLOGIES





## SECTION 8. DEVELOPMENT TYPOLOGIES

The San Marco Avenue entry corridor has two distinct characters. The area south of Picolata Road presents a mix of buildings with a historic character, some originally designed for residential use while others were originally intended for commercial use. The area north of Picolata Road introduces industrial/utilitarian buildings and more modern structures. Both areas contain institutional buildings (e.g. the historic Water Works building, the National Guard Armory, the Florida School for the Deaf and the Blind). Future infill and redevelopment is required to perpetuate the characteristics of each area by adopting and/or reinforcing one of the following building typologies.

### 8.1 Residential Typology

Even though the name and appearance of this typology refers to a residential use, any of the permitted uses in the zoning district may utilize this design if located in the area **south of Picolata Road**. Where a building exists that is considered a legacy building, redevelopment shall maintain the character features of the building and site arrangement.

The reason for allowing and encouraging the use of this development typology is to preserve the current character of the south area of the San Marco Avenue corridor, which mixes residential and commercial development types in a single block.

Typical characteristics of the current Residential Typology include two story to two and one-half story buildings setback from the street at least ten feet, allowing room for landscaping and a front porch. The main entrance faces the street, and windows are traditionally vertical in orientation and include top and bottom panes (single/double hung). Building materials typically include wood siding and in a few instances, stucco. Roof design is sloped in gable or hip form, and usually finished with shingles or metal.

While replicas of buildings are not allowed, new development proposing to use this typology, additions, and modifications will be required to respect the main features of this type of building. New developments and renovations of existing residential conversions using this typology will be eligible for a waiver of parking space requirements with a reduction of overall parking by 25% (except for handicap spaces), and will be eligible for additional signage (see Section 8). This reduction to the required parking will aid in maintaining a 'front yard' with an adequate setback characteristic of the residential typology.



*Single family homes converted to commercial use  
(expansive setbacks, two stories, porches)*



## 8.2 Commercial Typology

This typology refers to the commercial buildings of the early twentieth century found along San Marco Avenue. This typology may be used for new development, additions and modifications anywhere along the corridor and for any use permitted in the zoning district. Where a building exists that is considered a legacy building, redevelopment shall maintain the character features of the building and site arrangement.

The main characteristics of this typology include one to two story buildings with retail uses occupying the ground floor and residential or commercial uses above. The ground floor usually includes storefront windows and glass doors and a canopy or awning. The buildings are typically placed close to the public sidewalk. Building materials are typically masonry (brick, stucco). Roofs are typically flat and include a parapet. Developments in the south district (south of Picolata Road) using this typology will be required to meet the architectural standards based on the traditional design of such buildings. Developments in the north district (north of Picolata Road) may utilize more modern architectural styles but will still be required to meet the general building design standards detailed later in this document.

With the advent of the automobile, development along the San Marco Avenue corridor started to deviate from the Residential and the Commercial typologies mentioned above and started introducing auto-oriented uses. Auto-oriented site/building designs are not allowed.

Minor additions and modifications will need to be consistent with the existing building's typology, but substantial improvements (as defined in Section 3) will require the conversion of the existing building typology into the Commercial Typology described above.



*Current and encouraged site/building typology along San Marco Avenue south of Picolata Road*



*Public, civic and institutional buildings are intended to stand out (unique architectural design, setbacks that accommodate public gathering and respond to individual needs of use). Specific typology styles for existing institutions are defined below. These can only be used by institutional use properties.*

## 8.3 Institutional Typology

Public, civic and institutional buildings are intended to stand out and to serve as landmarks in the community. For that reason, they typically feature a different building type and style than their neighboring buildings and it is important to establish a different set of development and design parameters for this type of facility. The placement on the site, for instance, must be able to accommodate the gathering of large groups of people. This typology may only be used for public, civic and institutional buildings.

Institution	Architectural Style*
Mission Nombre de Dios	Contemporary/Modern
Waterworks Building	Masonry Vernacular
National Guard Armory	Mission Revival
Florida School for the Deaf and the Blind	Mediterranean Revival



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## APPENDICES



## APPENDIX A: ARCHITECTURE

### FRAME VERNACULAR

#### BACKGROUND

Frame Vernacular is one of the most dominant architectural styles in the San Marco Avenue area. Vernacular architecture refers to a regional or “folk” architecture, built with local materials and local labor, without formal plans, and for the most economical price at the time. Vernacular, while considered a style, does not belong to any particular formal architectural style, rather its design reflects a trend toward simplicity and its character is adapted to local conditions. Some Frame Vernacular buildings have one story, others have two; while some have front gable roofs, others may have side-gable or cross gable roofs. Most Frame Vernacular buildings in the district are residential homes; however, the style is often used on non-residential buildings as well.

#### BUILDING PLAN

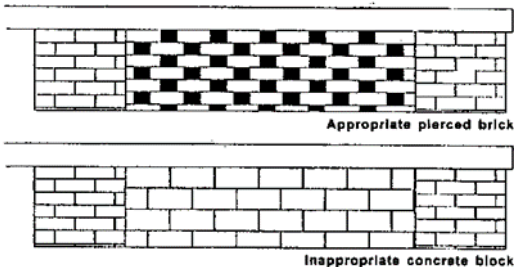
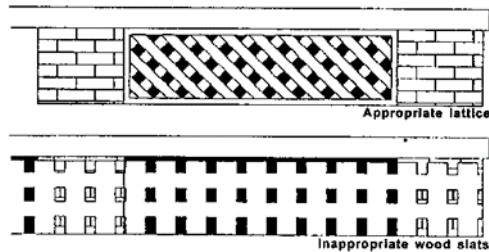
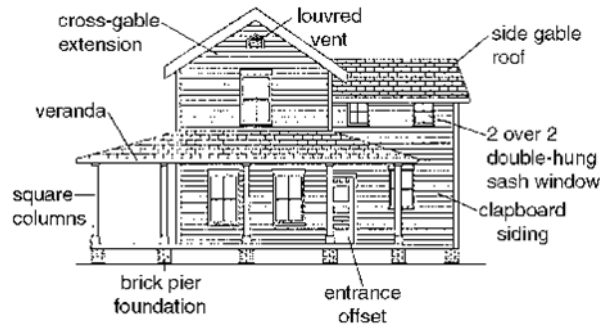
1. Rectangular or L-shaped.
2. Garages and carports are secondary to the main building/facade. They are either recessed from main façade or detached and placed in the rear. Sites that have been converted to commercial or office use have demolished the garage (if one was present in the rear) and replaced it with surface parking.

#### FOUNDATION

1. Brick, coquina or concrete block piers.
2. Spaces between piers left open.
3. Lattice infill between piers is common.

#### FACADES

1. Vertically oriented.





2. Three-bay or five-bay pattern.
3. Simple entrance.

## COLORS

1. Colors range in the pastel family from light yellows and grays to light pastel colors.

## PORCHES

1. Wide front porches.
2. Full width, wrap around, or fill in between the “L”.
3. Commonly elevated 2’-6” to 3’-6” above grade.
4. Columns are typically narrow and made of wood with few details; usually spaced evenly across the façade.
5. If railings used, they were wooden with 1 ¼ inch square balustrades.

## ROOF

1. Front, side and cross gable.
2. Main roofs are steeply pitched (8:12 to 12:12).
3. Porch roofs have a low pitch (2:12 to 4:12).
4. Rafter ends are exposed, unadorned, and extend beyond the face of the wall.
5. Asbestos, composition or metal shingles, V-crimp metal.
6. Brick or stucco chimneys, typically on the gable end walls.
7. Dormers sometimes used on 1 ½ story homes.

## DOORS

1. Transoms, fanlights are common.
2. Door trim projects out from wall cladding, approximately ¾ inch. Jamb trim min. 4” wide, and headers (lintel) are normally 6” wide.
3. Doors contain recessed wood panels.

## WINDOWS

1. Windows are single or in pairs, tall and narrow.



## LOCAL EXAMPLES





## City of St. Augustine San Marco Avenue Design Standards



2. Multi-pane, double-hung sash windows (6/6, 3/1, 2/2 or 1/1). Jalousie used in later construction (mid-century).
3. Transoms, fanlights and attic louvers are common.
4. Second floor windows align with first floor windows.
5. Houses built in the early 1900s had wood windows. Aluminum and vinyl windows have been used more recently as replacements.
6. Windows are spaced evenly along all facades.
7. Window trim projects out from wall cladding, approximately  $\frac{3}{4}$  inch. Jamb trim min. 4" wide, and headers (lintel) typically 6" wide.
8. Shutters are not typically used. When used, they should be paneled and should be either operable or proportioned to look operable.

### MATERIALS

1. Horizontal wood siding; less common are wood shingles and board and batten. Vinyl and fiber cement siding used more recently.

### ORNAMENTATION

1. Sparse, limited to ornamental woodwork.

### FENCES

1. Wooden fences are most common.
2. Picket fences with various level of ornamentation, consistent with the main building.



### LOCAL EXAMPLES



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## LOCAL EXAMPLES

# TRADITIONAL COMMERCIAL

## BACKGROUND

Generally, a one-story building formed by a structural framework consisting of columns, bulkheads or kick-panels, and a cornice topped by a parapet. Large show windows generally placed within this framework to display merchandise and light the interior. The wall area between the windows and the cornice provide a place for advertising and makes the façade appear taller. In the case of multi-story commercial buildings, it is organized into an upper and lower zone. The design of the lower zone was essentially the same as the one story building. The lower zone generally houses commercial uses, while the upper zone often provided space for private uses, such as apartments, offices, hotel rooms, and meeting halls.

## BUILDING PLAN

- 1) Rectangular plan.
- 2) One to two stories in height.

## FOUNDATION

- 1) Continuous concrete slab or brick foundation.

## FAÇADE

- 1) Narrow front elevation facing the street.
- 2) Focus of the design. Provides the building's identifying features.
- 3) Different framework in upper and lower stories.

## ROOF

- 1) Flat with parapet or hidden, pitched roof.

## MATERIALS

- 1) Brick; also masonry units and other unique materials like coquina concrete, cast stone
- 2) Primary exterior material is brick with common or running bond.
- 2) Rough textured stucco.

## WINDOWS AND DOORS (STOREFRONT)

- 1) Fixed plate glass display windows and doors on the first floor.
- 2) Double-hung sash windows in the upper floors.

## COLORS

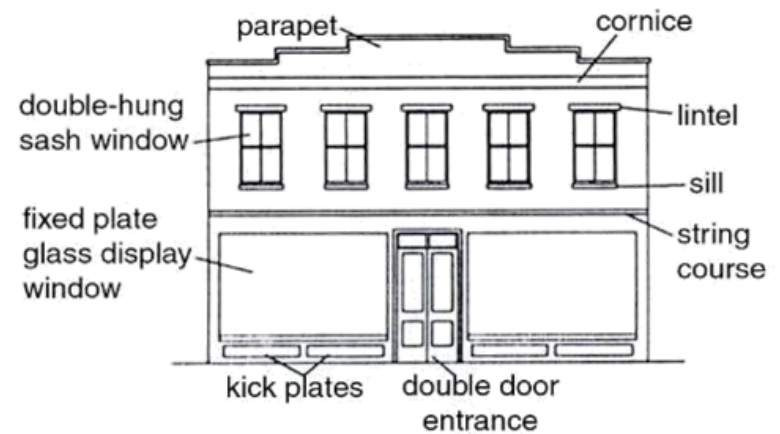
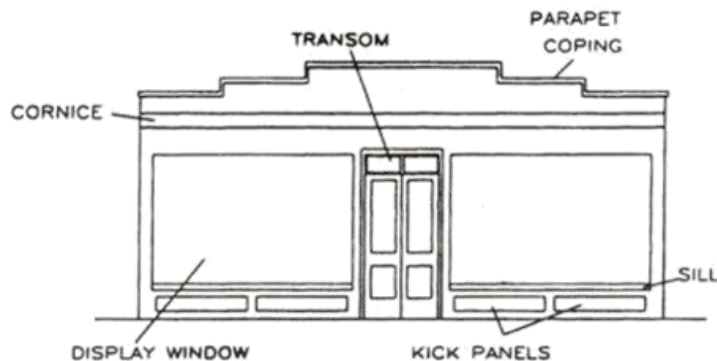
- 1) Exposed brick with trim colors ranging from earth tones to pastels.

## ORNAMENTATION

- 1) Awnings, canopies.
- 2) Cornice.
- 3) Cast concrete detailing.
- 4) Decorative brick work, such as corbeling.

## FENCES

- 1) No fences should be placed in the front between the right of way and the building face.







## APPENDIX B: LANDSCAPE PLANTS

### Botanical Name

### Common Name

#### Street Trees

<i>Quercus laurifolia</i>	Laurel Oak
<i>Quercus virginiana</i>	Live Oak
<i>Quercus shumardii</i>	Shumard Oak
<i>Plantanus occidentalis</i>	Sycamore
<i>Taxodium distichum</i>	Bald Cypress
<i>Ulmus parrifolia</i>	Drake Elm

#### Street Palms

<i>Phoenix canariensis</i>	Canary Island Date Palm
<i>Phoenix dactalifera</i>	Date Palm
<i>Phoenix sylvestris</i>	Sylvestris Date Palm
<i>Sabal palmetto</i>	Sabal Palm
<i>Washingtonia robusta</i>	Washington Palm

### Botanical Name

### Common Name

#### Accent Trees and Palms

<i>Acer rubrum</i>	
<i>Bambusa</i> spp.	Red Maple
<i>Butia capitata</i>	Bamboo (clumping)
<i>Cercis cananadensis</i>	Pindo Palm
<i>Ilex cassine</i>	Redbud
<i>Ilex opaca</i>	Dahoon Holly
<i>Ilex vomitoria</i>	American Holly
<i>Juniperus silicicola</i>	Yaupon Holly
<i>Juniperus virginiana</i>	Southern Red Cedar
<i>Lagerstroemia indica</i>	Eastern Red Cedar
<i>Ligustrum</i> spp.	Crape Myrtle
<i>Liquidambar styraciflua</i>	Ligustrum
<i>Livistonia chinensis</i>	Sweetgum
<i>Magnolia grandiflora</i>	Chinese Fan Palm
<i>Myrica cerifera</i>	Southern Magnolia
<i>Phoenix reclinata</i>	Wax Myrtle
<i>Prunus agustifolia</i>	Senegal Date Palm
<i>Prunus calleryana</i>	Chickasaw Plum
<i>Prunus caroliniana</i>	Bradford Pear
<i>Trachycarpus fortunei</i>	Cherry Laurel
	Windmill Plum



## Botanical Name

## Common Name

### Hedges\*, Shrubs\*, Cycads and Palms

(\*To be used as a screen for parking lots.)

Abelia grandiflora*	Abelia
Camellia japonica	Camellia
Camellia sasanqua	Sasanqua Camellia
Cassia spp.	Cassia
Cortaderia selloana	Pampas Grass
Crinum spp.	Crinum Lily
Eleagnus pungens*	Silverthorn
Eragrostis spectabilis	Purple Lovegrass
Feijoa sellowiana*	Feijoa
Galphimia glauca	Thryallis
Gardenia jasminoides	Gardenia
Hibiscus spp.	Hibiscus
Ilex cassine	Dahoon Holly
Ilex cornuta*	Chinese Holly
Ilex cornuta* "Burfordi"	Burford Holly
Ilex crenata	Japanese Holly
Ilex vomitoria "nana"	Dwarf Yaupon Holly
Illicium anisatum*	Star Anise
Illicium parviflorum*	Yellow Anise
Juniperus chinensis*	Chinese Juniper
Juniperus spp.	Juniper
Lantana camara	Lantana
Ligustrum japonicum*	Japanese Privet
Magnolia grandiflora 'Little Gem'	Little Gem Magnolia
Muhlenbergia capillaris	Muhly Grass
Nandina domestica	Heavenly Bamboo

## Botanical Name

## Common Name

### Hedges\*, Shrubs\*, Cycads and Palms

Nerium oleander	Oleander
Philodendron selloum	Split Leaf Philodendron
Photinia fraseri	Fraser's Photinia
Pittosporum tobira*	Pittosporum
Plumbago auriculata	Plumbago
Podocarpus macrophyllus*	Podocarpus
Pyracantha coccinea	Firethorn Pyracantha
Raphiolepis indica	Indian Hawthorn
Rhapidothylus hystrix	Needle Palm
Rhododendron indica*	Indica Azaleas
Rhododendron obtusum	Dwarf Azaleas
Rhododendron kurume	Kurume Azaleas
Serenoa repens	Saw Palmetto
Spartina spp.	Cordgrass
Thuja occidentalis	American Arborvitae
Viburnum odoratissimum*	Sweet Viburnum
Viburnum suspensum*	Sandankwa Viburnum
Viburnum Tinus	Laurustikus Viburnum
Yucca elephantipes	Spineless Yucca
Yucca smalliana	Bear Grass
Zamia floridana	Coontie
Zamia furfuracea	Cardboard Plant



## APPENDIX B : LANDSCAPE PLANTS

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<i>Myrica cerifera</i> ( <i>Morella cerifera</i> )	Wax Myrtle
<i>Phoenix reclinata</i>	Senegal Date Palm
<i>Prunus agustifolia</i>	Chickasaw Plum
<i>Prunus calleryana</i>	Bradford Pear
<i>Prunus caroliniana</i>	Cherry Laurel
<i>Trachycarpus fortunei</i>	Windmill Plum



Botanical Name Common Name

**Hedges\*, Shrubs\*, Cycads and Palms**

(\*To be used as hedge or screen for parking lots.)

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Viburnum Tinus	Laurustikus Viburnum
Yucca elephantipes	Spineless Yucca
Yucca smalliana	Bear Grass
Zamia floridana	Coontie
Zamia furfuracea	Cardboard Plant





## APPENDIX C COLORS



SW 0036  
**Buckram Binding**  
Interior



SW 0035  
**Indian White**  
Interior / Exterior



SW 0037  
**Morris Room Grey**  
Interior / Exterior



SW 0040  
**Roycroft Adobe**  
Interior / Exterior



SW 6316  
**Rosy Outlook**  
Interior / Exterior



SW 6771  
**Bathe Blue**  
Interior / Exterior



SW 6464  
**Aloe**  
Interior / Exterior



SW 6815  
**Awesome Violet**  
Interior / Exterior



SW 0041  
**Dard Hunter Green**  
Interior / Exterior



SW 0043  
**Peristyle Brass**  
Interior / Exterior



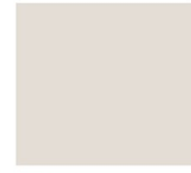
SW 0048  
**Bunglehouse Blue**  
Interior / Exterior



SW 0074  
**Radiant Lilac**  
Interior / Exterior



SW 0025  
**Rosedust**  
Interior / Exterior



SW 6035  
**Gauzy White**  
Interior / Exterior



SW 7083  
**Darkroom**  
Interior / Exterior



SW 0065  
**Vogue Green**  
Interior / Exterior



SW 0066  
**Cascade Green**  
Interior / Exterior



SW 0067  
**Belvedere Cream**  
Interior / Exterior



SW 0068  
**Copen Blue**  
Interior / Exterior



SW 6401  
**Independent Gold**  
Interior / Exterior



SW 6556  
**Obi Lilac**  
Interior / Exterior



SW 0069  
**Rose Tan**  
Interior / Exterior



SW 0070  
**Pink Shadow**  
Interior / Exterior



SW 0071  
**Orchid**  
Interior / Exterior



SW 0072  
**Deep Maroon**  
Interior

NOTE: Colors may be increased or decreased in saturation 10% from color sample.



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## APPENDIX D: LEGACY BUILDINGS

Buildings marked in pink are deemed Legacy Buildings by the COSA Historic Preservation Officer based upon the presence of traditional building materials and original form and plan of building is greater than or equal to 50 years old. For addresses, see the list included on the following pages



City of St. Augustine  
San Marco Avenue Design Standards



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City of St. Augustine  
San Marco Avenue Design Standards

58 San Marco Ave.



76 San Marco Ave.



60 San Marco Ave.



82 San Marco Ave.



74 San Marco Ave.



102 San Marco Ave.





138 San Marco Ave.



208 San Marco Ave.



154 San Marco Ave.



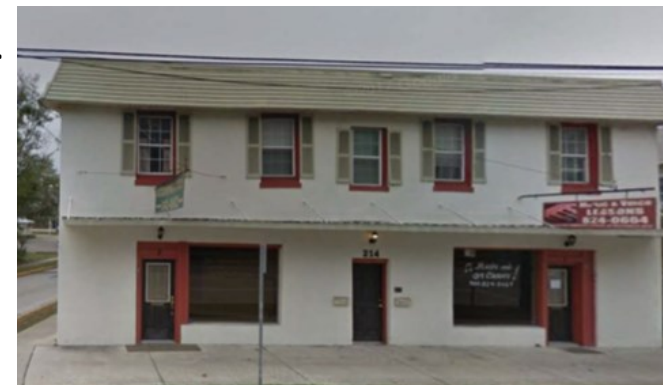
210 San Marco Ave.



172 San Marco Ave.



214 San Marco Ave.







City of St. Augustine  
San Marco Avenue Design Standards

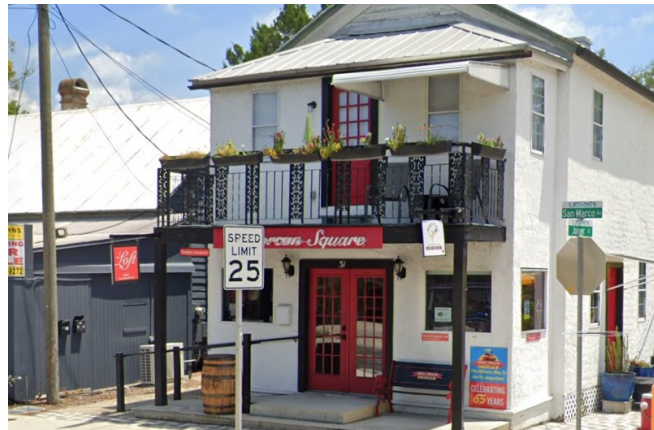
236 San Marco Ave.



39 San Marco Ave.



31 San Marco Ave.



41 San Marco Ave.



35 San Marco Ave.



45 San Marco Ave.





63 San Marco Ave.



75 San Marco Ave.



67 San Marco Ave.



77 San Marco Ave.



73 San Marco Ave.



81 San Marco Ave.







City of St. Augustine  
San Marco Avenue Design Standards

147 San Marco Ave.



197 San Marco Ave.



155 San Marco Ave.



239 San Marco Ave.



195 San Marco Ave.



299 San Marco Ave.



City of St. Augustine  
**San Marco Avenue Design Standards**





## APPENDIX E: LEGAL NON-CONFORMING TYPOLOGIES

The following typologies are defined for use on existing buildings meeting this description. All other building types and new construction may not replicate or construct buildings in this typology.

### Auto Service Type

The Auto Service Typology reflects a few small scale gas stations and vehicle repair buildings along San Marco Avenue, which in some cases date back to the 1930s and are a character feature of San Marco Avenue. This architectural typology is not suitable or permissible for new development, and is only applicable when reusing or repurposing existing auto service type buildings

The existing buildings are placed at a distance from the sidewalk, to allow vehicles to easily enter and exit the site. The presence of automobiles in front of the building (circulating or parked) is not conducive to pedestrian activity in the corridor. However, the continued use and the conversion of the existing buildings to accommodate other uses permitted in the zoning district will be allowed in order to preserve the existing buildings. When conversions are proposed, the main features of this type of development such as the large bay openings (may be converted to large window openings) will need to be preserved, but the vehicle activity in the front will be limited. Additions to the front of the building will not be allowed, as they would detract from the historic character of such buildings.

### Mid-Century Lodging

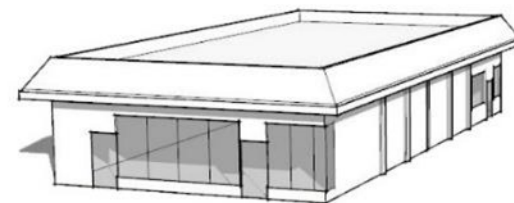
The Mid-Century Lodging typology reflects a few existing motels along San Marco Avenue, which were very common in Florida in the 1950s. The city intends to encourage their preservation and rehabilitation because they are a character feature of San Marco Avenue. This architectural typology is not suitable or permissible for new development in the San Marco corridor and is only applicable when reusing or repurposing existing mid-century lodging type buildings. New lodging developments shall choose the Residential or Commercial Development typologies. The features of this typology that should be preserved include the courtyard site layout with one or two stories of rooms with access from the courtyard. The buildings typically surround a central area that includes parking and amenities (swimming pool).



## Suburban Type

The Suburban typology also reflects an existing type of development which started appearing in the San Marco Avenue corridor in the 1970s, prior to the adoption of design standards for the entry corridors. This type of development is not compatible with the urban, pedestrian-friendly character of the San Marco Avenue corridor. Therefore, this typology shall not be used for new development along the corridor. It is mentioned as one of the typologies found in the district for the purpose of guiding future additions and modifications to existing sites that already display this design. Redevelopment on these sites shall follow one of the other typologies allowed.

Developments in this typology consist of small buildings (relative to the size of the lot) placed at a large distance from the public sidewalk to allow for parking in front of the building. The architectural styles are very simple with no ornamentation or quality of design, large windows and mansard roofs. These buildings, because of their placement far from the street, rely on large freestanding signs in the front yard.







City of St. Augustine  
**San Marco Avenue Design Standards**

Approved; June 25, 2018

Reformatted: November 25, 2020

**Prepared by**



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*Note that the Entry Corridor logo is a registered trademark of the City of St. Augustine, registered on December 22, 2020 with the State of Florida*